



**PSP42.2  
BLACK FOREST ROAD SOUTH | PRECINCT STRUCTURE PLAN**

April 2013

Future PSP

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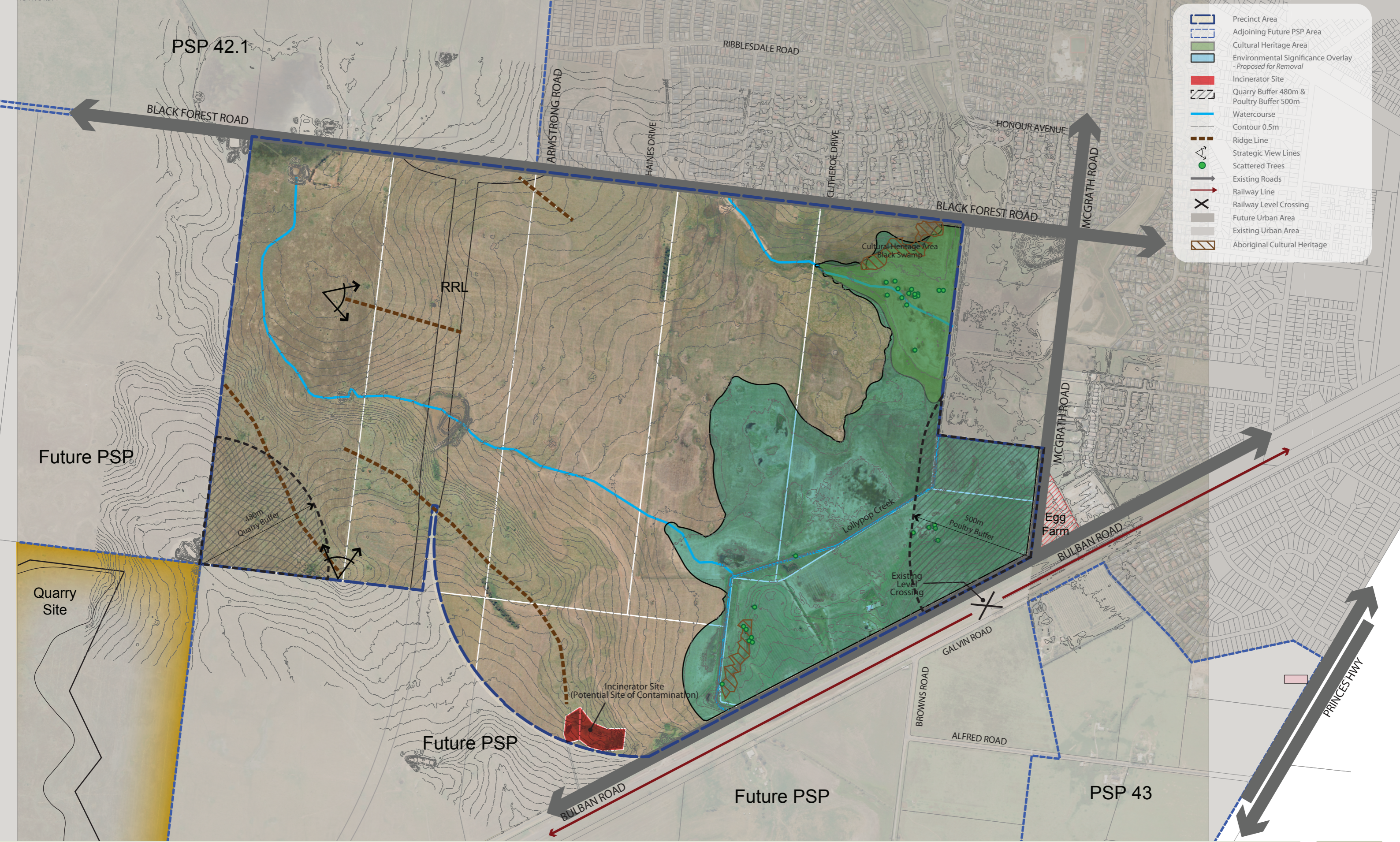
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- Precinct Area
- Adjoining Future PSP Area
- Cultural Heritage Area
- Environmental Significance Overlay - Proposed for Removal
- Incinerator Site
- Quarry Buffer 480m & Poultry Buffer 500m
- Watercourse
- Contour 0.5m
- Ridge Line
- Strategic View Lines
- Scattered Trees
- Existing Roads
- Railway Line
- Railway Level Crossing
- Future Urban Area
- Existing Urban Area
- Aboriginal Cultural Heritage

**Plan 1**  
 Scale: 1:12,500 @ A3 Date: April 2013  
 0 100 200 400m

## 1.0 INTRODUCTION

Black Forest Road South Precinct Structure Plan (PSP) has been prepared by the Growth Areas Authority (GAA) with the assistance of Wyndham City, Government agencies, service authorities and major stakeholders.

The PSP is a long-term plan for urban development. It describes how the land is expected to be developed, and how and where services are planned to support development.

The PSP:

- Sets out plans to guide the delivery of quality urban environments in accordance with Victorian Government guidelines.
- Enables the transition of non-urban land to urban land.
- Sets the vision for how the land should be developed and the outcomes to be achieved.
- Outlines the projects required to ensure that future residents, visitors and workers within the area can be provided with timely access to services and transport necessary to support a quality and affordable lifestyle.
- Sets out objectives, requirements and guidelines for land use and development.
- Provides Government agencies, the Council, developers, investors and local communities with certainty about future development.
- Addresses the requirements of the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act 1999) in accordance with an endorsed program under Part 10.

The PSP is informed by:

- The State Planning Policy Framework set out in the Wyndham Planning Scheme;
- The Melbourne West Growth Corridor Plan, June 2011;
- The Local Planning Policy Framework of the Wyndham Planning Scheme;
- The draft Biodiversity Conservation Strategy and Sub Regional Species Strategies for Melbourne's Growth Areas (DSE, 2011) and
- The Precinct Structure Planning Guidelines.

The following planning documents have been developed in parallel with the PSP to inform and direct the future planning and development of the locality:

- Black Forest Road South Native Vegetation Precinct Plan (NVPP) which sets out requirements for the protection and management of native vegetation within the PSP area.
- Draft Wyndham West Development Contributions Plan (DCP) that applies the requirements for development proponents to make a contribution toward infrastructure required to support the development of the precinct.

- Wyndham West Background Report (Background Report).

### 1.1 HOW TO READ THIS DOCUMENT

This precinct structure plan guides land use and development where a planning permit is required under the Urban Growth Zone or another zone where that zone references this precinct structure plan.

A planning application and a planning permit must implement the **OUTCOMES** of the precinct structure plan. The outcomes are expressed as the **VISION** and **OBJECTIVES**.

Each element of the precinct structure plan contains **REQUIREMENTS** and **GUIDELINES** as relevant.

**REQUIREMENTS** must be adhered to in developing the land. Where they are not demonstrated in a permit application, requirements will usually be included as a condition on a planning permit whether or not they take the same wording as in this structure plan. A requirement may include a reference or plan, table or figure in the precinct structure plan.

**GUIDELINES** express how discretion will be exercised by the responsible authority in certain matters that require a planning permit. If the responsible authority is satisfied that an application for an alternative to a guideline implements the outcomes the responsible authority may consider the alternative. A guideline may include or reference a plan, table or figure in the precinct structure plan.

Meeting these **REQUIREMENTS** and **GUIDELINES** will implement the outcomes of the precinct structure plan.

Development must also comply with other Acts and approvals where relevant e.g. the *Environment Protection and Biodiversity Conservation Act 1999* in the case of biodiversity or the *Aboriginal Heritage Act 2006* in the case of cultural heritage amongst others.

Not every aspect of the land's use and development is addressed in this structure plan and a responsible authority may manage development and issue permits as relevant under its general discretion.

### 1.2 LAND TO WHICH THIS PSP APPLIES

#### PLAN 1 – PSP LOCATION & FEATURES

The PSP applies to approximately **508** hectares of land in Werribee as shown on Plan 1. The PSP area is bound by McGrath Road to the east, Bulban Road to the south, Black Forest Road to the north, future urban land to the west and the future Melbourne – Geelong Rail Spur alignment to the south west.

Plan 1 also identifies the key features of the land.

### 1.3 DEVELOPMENT CONTRIBUTIONS PLAN

Development proponents within the Black Forest Road South precinct will be bound by the Wyndham West Development Contributions Plan (DCP). The DCP sets out requirements for infrastructure funding across the wider Wyndham West region and will be prepared separately to the PSP. Wyndham City is the development and collection agency for the DCP.

Once complete, the DCP will be a separate document that will be incorporated in the Wyndham Planning Scheme. Development proponents wishing to commence works prior to the approval and gazettal of this instrument have the opportunity to enter into Section 173 agreements with Council to expedite contributions.

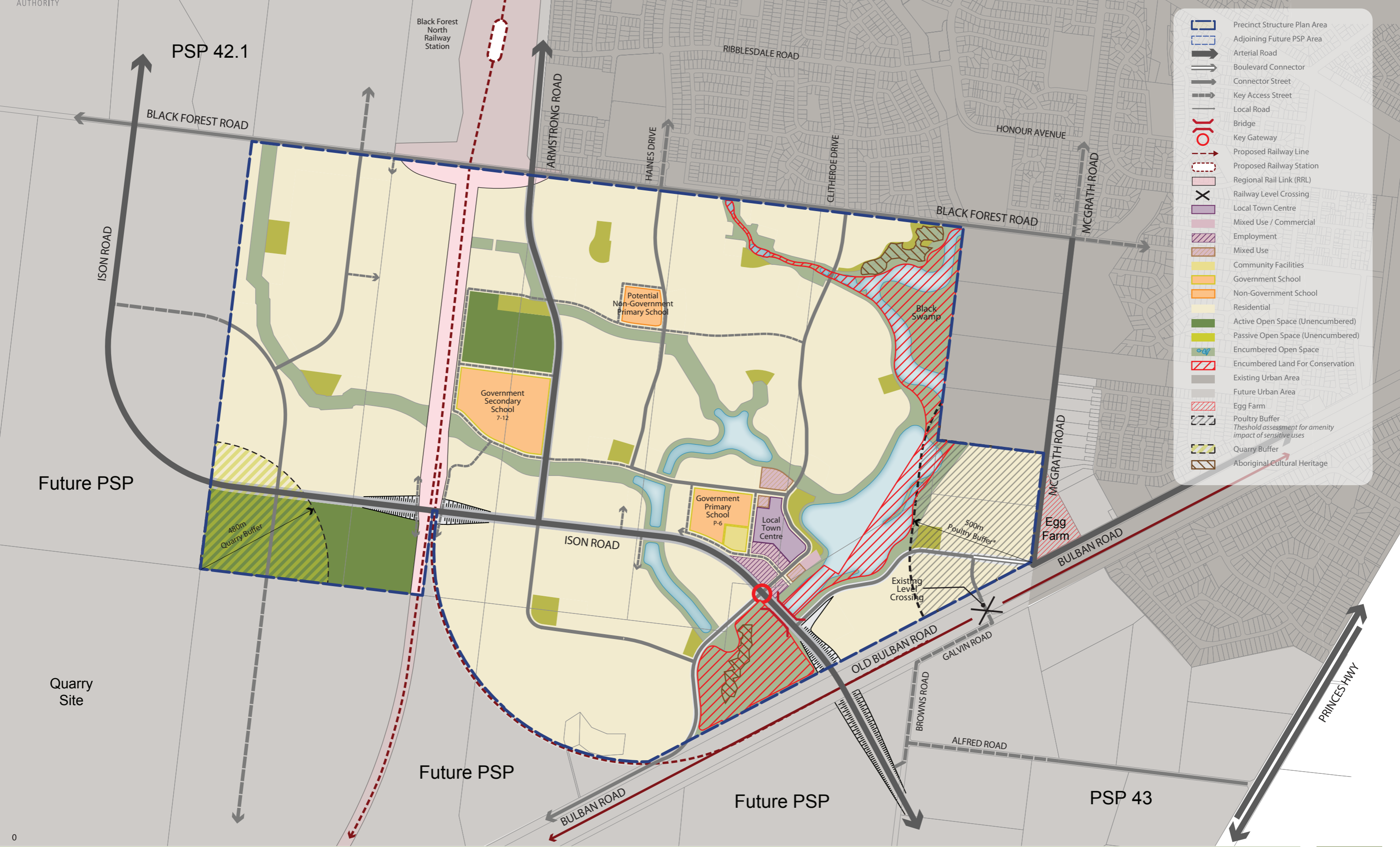
### 1.4 BACKGROUND INFORMATION

Detailed background information on the precinct – including its local and metropolitan context, history, landform and topography, drainage, biodiversity, open space and community facilities – are contained in background reports. This information has informed the preparation of the PSP.

The Black Forest Road South Precinct has been planned concurrently with the remainder of the Urban Growth Zone land contained within the Wyndham West growth front. The seven precincts in the wider Werribee West area incorporate approximately 2,500 hectares and will provide approximately 30,000 new dwellings and a forecast population of 80,000 people.

The Regional Rail Link (RRL) is the spine to the future Wyndham West communities and will deliver a new rail station for Manor Lakes and allows for a future rail station at Black Forest Road. Ison Road will ultimately provide the regional road through the area and will be a six lane arterial road managed by VicRoads. Ison Road will traverse from the Princes Freeway in the south crossing the Melbourne Geelong railway, the RRL and the Werribee River connecting with Dohertys Road in Tarneit. The Ison Road crossing of the Melbourne-Geelong Railway line is the critical piece of state infrastructure that will enable the growth and development of this precinct and the broader communities of Wyndham West.

Within the eastern portion of the precinct, significant areas of open space for conservation purposes and recreation use are proposed. The topography and landform of the precinct has resulted in this area being unique in terms of the potential to enhance threatened fauna habitat (Growling Grass Frog) and also to provide a range of active and passive recreational uses.



- Precinct Structure Plan Area
- Adjoining Future PSP Area
- Arterial Road
- Boulevard Connector
- Connector Street
- Key Access Street
- Local Road
- Bridge
- Key Gateway
- Proposed Railway Line
- Proposed Railway Station
- Regional Rail Link (RRL)
- Railway Level Crossing
- Local Town Centre
- Mixed Use / Commercial
- Employment
- Mixed Use
- Community Facilities
- Government School
- Non-Government School
- Residential
- Active Open Space (Unencumbered)
- Passive Open Space (Unencumbered)
- Encumbered Open Space
- Encumbered Land For Conservation
- Existing Urban Area
- Future Urban Area
- Egg Farm
- Poultry Buffer
- Threshold assessment for amenity impact of sensitive uses
- Quarry Buffer
- Aboriginal Cultural Heritage

## Plan 2

Scale: 1:12,500 @ A3 Date: April 2013  
 0 100 200 400m

Future Urban Structure

## 2.0 OUTCOMES

### 2.1 VISION

The Black Forest Road South Precinct will be a new residential community that integrates with existing residential neighbourhoods and provides future residents with opportunities to live, work, shop and play locally.

Underpinning the structure of the community will be Lollypop Creek and its tributaries that traverse the site. They will be transformed into a series of water bodies, wetlands and waterway corridors, providing future residents with a range of leisure opportunities. The amenity of the precinct will be further enhanced by strong landscaping of public spaces. The Black Swamp area will be retained and enhanced, ensuring the preservation of cultural and biodiversity assets.

The precinct will provide a variety of local parks that will integrate with the water bodies, wetlands and the lineal waterway corridors. Additionally, the precinct will have a range of community facilities including two active sports grounds, state secondary and primary schools and a non-government primary school.

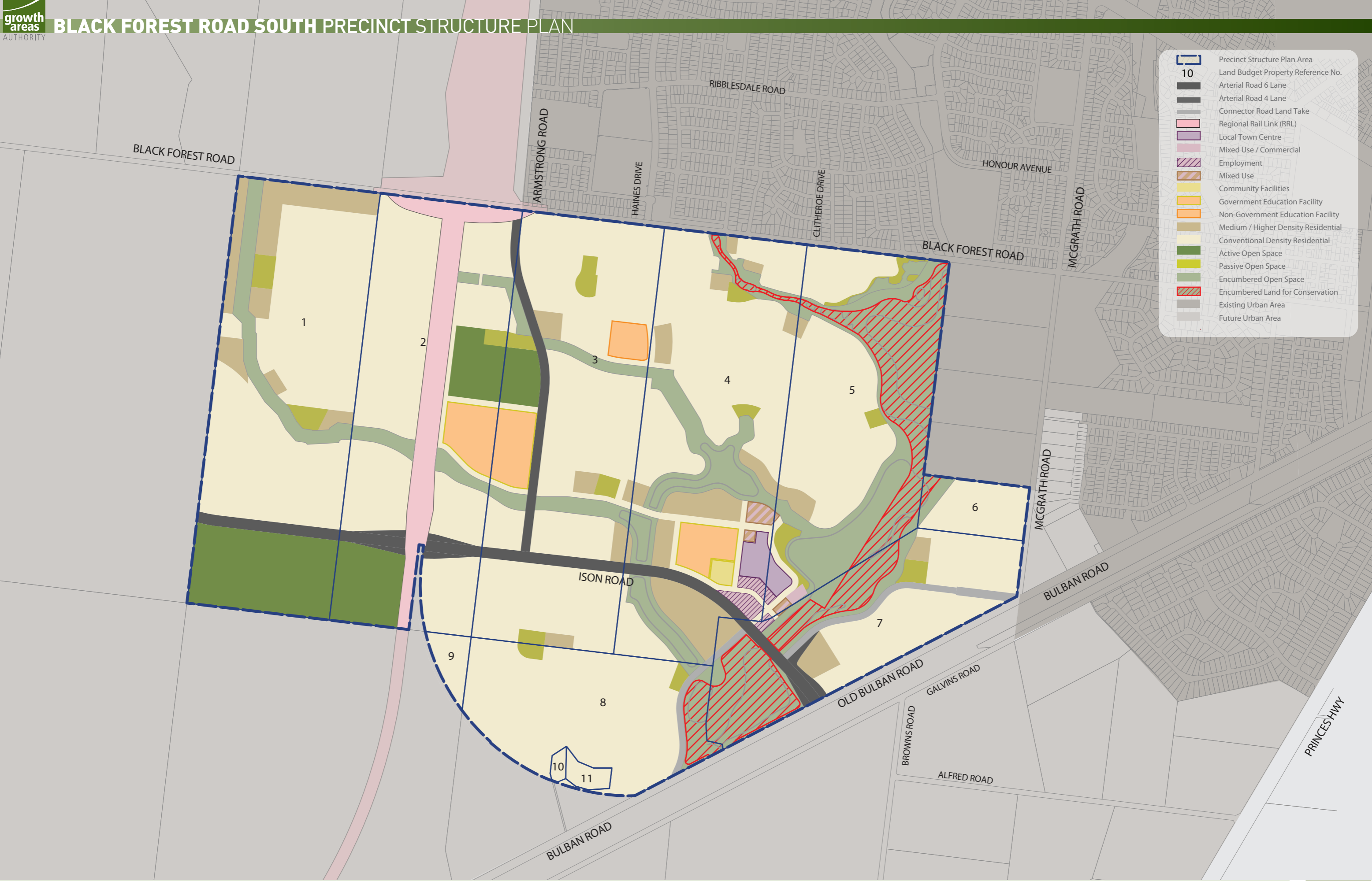
The road network will create a connected community through a hierarchy of tree lined streets that link the residents to open spaces. The southern length of Ison Road will provide a key gateway (emphasising built form and landscape treatments) to the Black Forest Road South precinct and the broader Wyndham West area.

The Local Town Centre will benefit from the adjacent open water body as well as its strategic location within the broader road network. This centre will be the heart of the community and consist of shops, community facilities and a state primary school as well as providing the opportunity for local employment.

The precinct will deliver a broad range of housing and encourage a diverse community. The Local Town Centre will also cater for medium to higher density housing. Multi-unit and small-lot housing will be located in places of high amenity and accessibility such as around open space and the Local Town Centre

### 2.2 OBJECTIVES

	To provide a highly permeable and connected community that:
<b>01</b>	<ul style="list-style-type: none"> <li>Has attractive streetscapes and landscape treatments and distinctive neighbourhood character.</li> <li>Provides high quality active and passive recreation spaces to encourage active lifestyles.</li> </ul>
<b>02</b>	To ensure waterway corridors and the central water bodies have active edges that provide the key linking elements of the open space network.
	To provide a variety of park types with diversity of experience and use throughout the precinct, with landscape designs that complement the proposed surrounding land use. Including:
<b>03</b>	<ul style="list-style-type: none"> <li>Natural landscape design to compliment ecological areas such as the Black Swamp and Lollypop Creek.</li> <li>Urbanised landscape design adjacent to the LTC and community hub.</li> </ul>
<b>04</b>	To ensure the design of the intersection of Ison Road and Bulban Road reflects its role as the key gateway to the precinct and Wyndham West.
<b>05</b>	To encourage non noise-sensitive land uses to be located adjoining the RRL corridor.
<b>06</b>	To protect the amenity of residential areas adjacent to the RRL corridor.
<b>07</b>	To develop a diverse residential community through a range of housing types and sizes, which will achieve an average of at least 15 dwellings per net developable hectares (NDHa).
<b>08</b>	To ensure dwellings are not adversely impacted upon by uses with adverse amenity potential and provide sufficient distance between sensitive uses and those with adverse amenity impact.
<b>09</b>	To ensure that development staging is co-ordinated with the delivery of key local and state infrastructure.
<b>010</b>	To create a LTC that meets the local retail and service needs of new residents.
<b>011</b>	To design a civic square and promenade area that links the open water bodies to the LTC.
<b>012</b>	To encourage the development of a broad range of business activity including small office, mixed use and home based businesses within and on the periphery of the LTC.
<b>013</b>	To co-locate active open space areas with community and education facilities and encourage the sharing of open space infrastructure between these uses.
<b>014</b>	To embrace the future water bodies and waterway corridors as the green spines of the precinct that provide links to parks, community facilities, town centres and public transport.
<b>015</b>	To plan for the long term conservation of significant vegetation and fauna habitat areas to be retained within the community.
<b>016</b>	To implement the requirements of the Growing Grass Frog Sub Regional Strategy in creating and protecting critical habitat for the Growling Grass Frog.
<b>017</b>	To preserve and enhance the biodiversity and cultural heritage values within the Black Swamp.
<b>018</b>	To maximise the quality and long term viability of vegetation in public areas through the use of Water Sensitive Urban Design (WSUD).
<b>019</b>	To deliver Integrated Water Management (IWM) systems that reduce reliance on reticulated potable water and increase the re-use of alternative water (storm and / or waste) and contribute to a sustainable and green urban environment.
<b>020</b>	To establish lots, to the satisfaction of the relevant authority, with potable water, electricity, reticulated sewerage, drainage, gas and telecommunications.
<b>021</b>	To recognise the strategic importance of the Ison Road crossing of the Melbourne-Geelong railway to facilitate the growth of the precinct beyond the early stages of its development. Ensure the subsequent intersection of Ison Road and Bulban Road is designed as a gateway to the precinct and the Wyndham West communities.
<b>022</b>	To allow and encourage alternative modes of travel by creating an efficient street network that links pedestrians and cyclists to the LTC, schools, open space, community facilities and public transport to neighbouring communities.
<b>023</b>	To provide a designated pedestrian and bicycle path along the RRL to provide connection to the future train station to the north of the precinct.
<b>024</b>	To provide an integrated, permeable and efficient street network that provides alternatives to the use of private vehicles and ensures good connectivity both within and beyond the precinct.
<b>025</b>	To identify areas where the bushfire hazard requires specified bushfire protection measures for subdivision and buildings and works to be implemented.
<b>026</b>	To ensure that the location, design and construction of development considers the need to implement bushfire protection measures.
<b>027</b>	To ensure development does not proceed unless the risk to life and property from bushfire can be reduced to an acceptable level.



	Precinct Structure Plan Area
	Land Budget Property Reference No.
	Arterial Road 6 Lane
	Arterial Road 4 Lane
	Connector Road Land Take
	Regional Rail Link (RRL)
	Local Town Centre
	Mixed Use / Commercial
	Employment
	Mixed Use
	Community Facilities
	Government Education Facility
	Non-Government Education Facility
	Medium / Higher Density Residential
	Conventional Density Residential
	Active Open Space
	Passive Open Space
	Encumbered Open Space
	Encumbered Land for Conservation
	Existing Urban Area
	Future Urban Area

## Plan 3

Scale: 1:12,500 @ A3 Date: April 2013  
 0 100 200 400m

## Land Use Budget



## 2.3 SUMMARY LAND BUDGET

### PLAN 3 – LAND BUDGET

The Net Developable Area (NDA) is established by deducting the land requirements for community facilities, public and private education facilities, and open space (active and passive) from the Total Precinct Area (GDA). The NDA for the Black Forest Road South Precinct is 330.56 hectares which equates to 65.13% of the total PSP area.

The land budget shows that the PSP achieves a lot density of approximately 16 dwellings per Net Developable hectare (NDHa).

The PSP will yield approximately 5,319 lots including over 800 lots less than 300 square metres.

Based on an average household size of 2.8 persons (Victoria in Future, 2012) the future population of the PSP is estimated at approximately 14,893 people.

*NOTE: All drainage infrastructure land take is indicative and additional land take may be required for drainage and water quality purposes.*

**Table 1: Summary Land Budget**

DESCRIPTION	RESIDENTIAL AREA		
	HECTARES	% OF TOTAL PREC	% OF NDA
<b>TOTAL PRECINCT AREA (ha)</b>	<b>509.87</b>	<b>100.0%</b>	
<b>TRANSPORT</b>			
Arterial Roads 6 Lanes & Embankment	12.09	2.37%	3.66%
Arterial Roads 4 Lanes	4.30	0.84%	1.30%
Black Forest Road Land Take	1.01	0.20%	0.31%
Bulban Road Land Take	3.96	0.78%	1.20%
Rail Reserve - PAO RRL	14.86	2.91%	4.50%
Rail Reserve - PAO Future Metro Spur	2.34	0.46%	0.71%
<b>Sub-total</b>	<b>38.56</b>	<b>7.56%</b>	<b>11.67%</b>
<b>COMMUNITY FACILITIES</b>			
Community Services Facilities	0.80	0.16%	0.24%
<b>Subtotal</b>	<b>0.80</b>	<b>0.16%</b>	<b>0.24%</b>
<b>EDUCATION</b>			
Government Schools	11.90	2.33%	3.60%
Non Government Schools	2.00	0.39%	0.61%
<b>Subtotal</b>	<b>13.90</b>	<b>2.73%</b>	<b>4.21%</b>
<b>OPEN SPACE</b>			
<b>Encumbered Land Available for Recreation</b>			
Waterway / Drainage Line / Wetland / retarding (encumbered)	49.81	9.77%	15.08%
Aboriginal Cultural Heritage Open Space (encumbered)	0.98	0.19%	0.30%
CMP Category 1 Open Space (encumbered)	34.40	6.75%	10.41%
<b>Sub-total Encumbered Land Potentially for Recreation</b>	<b>85.18</b>	<b>16.71%</b>	<b>25.78%</b>
<b>Unencumbered Open Space (std credited provision)</b>			
Active Open Space (DCP Delivered)	23.14	4.54%	7.00%
<b>Passive Open Space (52.01 Delivered)</b>	<b>9.91</b>	<b>1.94%</b>	<b>3.00%</b>
<b>Sub-total standard credited provision</b>	<b>33.05</b>	<b>6.48%</b>	<b>10.00%</b>
<b>Other Undetermined Open Space</b>			
Additional Active Open Space Provision (No compensation)	8.00	1.6%	2.42%
<b>Sub-total other unencumbered open space</b>	<b>8.00</b>	<b>1.6%</b>	<b>2.42%</b>
<b>Sub-total All Unencumbered Active Open Space</b>	<b>31.14</b>	<b>6.1%</b>	<b>9.43%</b>
<b>Sub-total All Open Space</b>	<b>126.23</b>	<b>24.8%</b>	<b>38.21%</b>
<b>NET DEVELOPABLE AREA (NDA) HA</b>	<b>330.38</b>	<b>64.80%</b>	

**Table 2: Estimated Dwelling Yield**

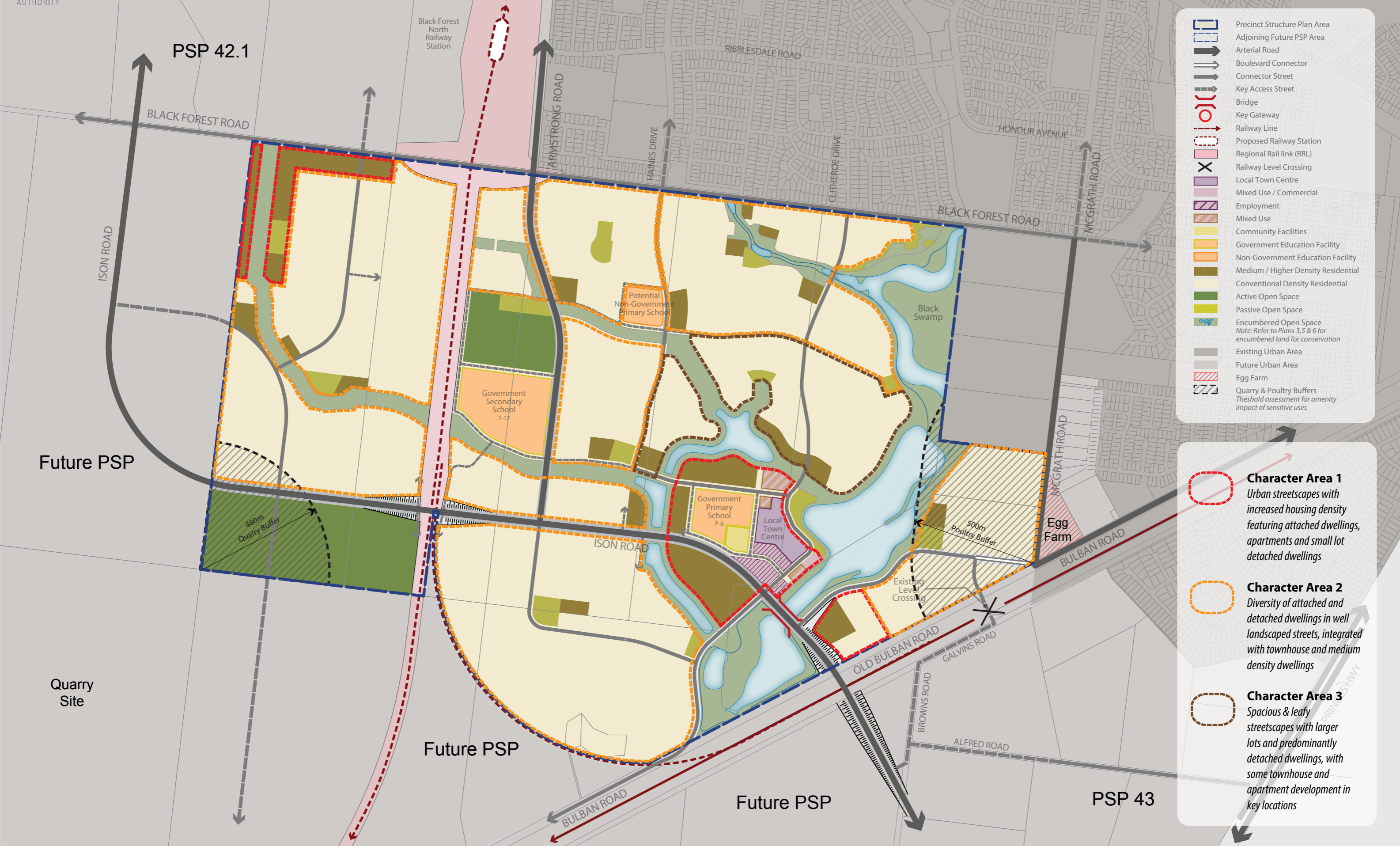
Residential	NDA(Ha)	Dwell / NDHa	Dwellings
Residential - Conventional Density Residential	294.47	15	4417
Residential - Medium / Higher Density	30.02	28	841
Local Town Centre Precinct	5.89	10	59
<b>Totals Residential Yield Against NDA</b>	<b>330.38</b>	<b>16.09</b>	<b>5317</b>

Table 3: Property Specific Land Use Budget

	Total Area (Hectares)	Transport & Utilities						Community Facilities	Education		Encumbered Land Available for Recreation			Unencumbered Open Space (Std credited provision)		Other Unencumbered Open Space	Total Net Developable Area (Hectares)	Key Percentages				52.01 Open Space Target %	Difference - Passive open Space Provision 52.01 delivery target (%)	Difference - Passive open Space Provision 52.01 delivery target (Ha)
		Westbrook Drive & Ison Road Arterial Road 6 Lane & Embankment (41m)	Armstrong Road Arterial Road 4 Lane (34m)	Black Forest Road - Additional Land Take (25m)	Bulban Road - Additional Land Take (23.6m & 32m)	Rail Reserve PAO RRL	Rail Reserve PAO Future Metro Spur		Government Schools	Non Government Schools	Waterway / Wetland / Retarding Open Space (encumbered)	Aboriginal Cultural Heritage Open Space (encumbered)	CMP Category 1 Open Space (encumbered)	Active Open Space (Delivered in DCP)	Passive Open Space (Delivered in 52.01)			Additional Active Open Space Provision (no compensation)	Net Developable Area % of Property/Precinct	DCP Delivered Active Open Space % NDA	Clause 52.01 Passive Open Space % NDA			
Property 1	86.72	2.21	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	6.59	0.00	0.00	12.14	1.97	4.00	59.54	68.66%	20.39%	3.31%	23.70%	3.00%	0.31%	0.18
Property 2	86.32	3.04	0.53	0.01	0.00	14.78	0.61	0.00	4.25	0.00	3.50	0.00	0.00	8.09	0.49	4.00	47.02	54.47%	17.21%	1.04%	18.25%	3.00%	-1.96%	-0.92
Property 3	86.68	2.21	3.77	0.25	0.00	0.08	0.00	0.00	4.15	2.00	5.22	0.00	0.00	2.91	2.69	0.00	63.40	73.14%	4.59%	4.24%	8.83%	3.00%	1.24%	0.79
Property 4	83.79	2.04	0.00	0.26	0.09	0.00	0.00	0.80	3.50	0.00	15.72	0.00	0.67	0.00	0.90	0.00	59.81	71.38%	0.00%	1.50%	1.50%	3.00%	-1.49%	-0.89
Property 5	65.60	0.00	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	12.87	0.98	17.37	0.00	2.15	0.00	32.02	48.81%	0.00%	6.71%	6.71%	3.00%	3.71%	1.19
Property 6	8.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.84	0.00	0.38	0.00	0.00	0.00	6.86	84.90%	0.00%	0.00%	0.00%	3.00%	-3.00%	-0.21
Property 7	46.06	2.59	0.00	0.00	2.80	0.00	0.00	0.00	0.00	0.00	4.32	0.00	13.38	0.00	0.80	0.00	22.17	48.13%	0.00%	3.61%	3.61%	3.00%	0.61%	0.14
Property 8	41.10	0.00	0.00	0.00	1.07	0.00	1.16	0.00	0.00	0.00	0.75	0.00	2.60	0.00	0.91	0.00	34.61	84.21%	0.00%	2.63%	2.63%	3.00%	-0.37%	-0.13
Property 9	3.36	0.00	0.00	0.00	0.00	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	2.81	83.63%	0.00%	0.00%	0.00%	3.00%	-3.00%	-0.08
Property 10	0.63	0.00	0.00	0.00	0.00	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.61	96.83%	0.00%	0.00%	0.00%	3.00%	-3.00%	-0.02
Property 11	1.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.53	100.00%	0.00%	0.00%	0.00%	3.00%	-3.00%	-0.05
<b>Total</b>	<b>509.87</b>	<b>12.09</b>	<b>4.30</b>	<b>1.01</b>	<b>3.96</b>	<b>14.86</b>	<b>2.34</b>	<b>0.80</b>	<b>11.90</b>	<b>2.00</b>	<b>49.81</b>	<b>0.98</b>	<b>34.40</b>	<b>23.14</b>	<b>9.91</b>	<b>8.00</b>	<b>330.38</b>	<b>64.80%</b>	<b>7.00%</b>	<b>3.00%</b>	<b>10.00%</b>	<b>3.00%</b>	<b>0.00%</b>	<b>0.00</b>

Table 4: Property Specific Dwelling Yield Guide

Property Number	Total Area (Hectares)	Total Net Developable Area (Hectares)	Other Land Uses					CONVENTIONAL DENSITY 15 Dwellings per NDHa		MEDIUM / HIGHER DENSITY 28 Dwellings per NDHa		Local Town Centre Precinct 10 Dwellings per NDHa		TOTAL COMBINED		
			Local Town Centre / Local Convenience Centre	Mixed Use / Commercial	Employment	Mixed Use	Total Residential Area Ha (NDA)	NDHa	Dwellings	NDHa	Dwellings	NDHa	Dwellings	NDHa	Dwellings/NDHa	Dwellings
Property 1	86.72	59.54	0.00	0.00	0.00	0.00	59.54	49.11	737	10.43	292	0.00	0	59.54	17.28	1029
Property 2	86.32	47.02	0.00	0.00	0.00	0.00	47.02	47.02	705	0.00	0	0.00	0	47.02	14.99	705
Property 3	86.68	63.40	0.00	0.00	0.00	0.00	63.40	60.26	904	3.14	88	0.00	0	63.40	15.65	992
Property 4	83.79	59.81	1.63	0.00	1.36	1.00	55.82	43.55	653	12.27	344	3.99	40	59.81	17.34	1037
Property 5	65.60	32.02	0.87	0.29	0.23	0.28	30.35	29.06	436	1.29	36	1.67	17	32.02	15.27	489
Property 6	8.08	6.86	0.00	0.00	0.00	0.00	6.86	6.86	103	0.00	0	0.00	0	6.86	15.01	103
Property 7	46.06	22.17	0.00	0.00	0.19	0.04	21.94	19.05	286	2.89	81	0.23	2	22.17	16.64	369
Property 8	41.10	34.61	0.00	0.00	0.00	0.00	34.61	34.61	519	0.00	0	0.00	0	34.61	15.00	519
Property 9	3.36	2.81	0.00	0.00	0.00	0.00	2.81	2.81	42	0.00	0	0.00	0	2.81	14.95	42
Property 10	0.63	0.61	0.00	0.00	0.00	0.00	0.61	0.61	9	0.00	0	0.00	0	0.61	14.75	9
Property 11	1.53	1.53	0.00	0.00	0.00	0.00	1.53	1.53	23	0.00	0	0.00	0	1.53	15.03	23
<b>Total</b>	<b>509.87</b>	<b>330.38</b>	<b>2.50</b>	<b>0.29</b>	<b>1.78</b>	<b>1.32</b>	<b>324.49</b>	<b>294.47</b>	<b>4417</b>	<b>30.02</b>	<b>841</b>	<b>5.89</b>	<b>59</b>	<b>330.38</b>	<b>16.09</b>	<b>5317</b>



	Precinct Structure Plan Area
	Adjoining Future PSP Area
	Arterial Road
	Boulevard Connector
	Connector Street
	Key Access Street
	Bridge
	Key Gateway
	Railway Line
	Proposed Railway Station
	Regional Rail link (RRL)
	Railway Level Crossing
	Local Town Centre
	Mixed Use / Commercial
	Employment
	Mixed Use
	Community Facilities
	Government Education Facility
	Non-Government Education Facility
	Medium / Higher Density Residential
	Conventional Density Residential
	Active Open Space
	Passive Open Space
	Encumbered Open Space <i>Note: Refer to Plans 3, 5 &amp; 6 for encumbered land for conservation</i>
	Existing Urban Area
	Future Urban Area
	Egg Farm
	Quarry & Poultry Buffers <i>Threshold assessment for amenity impact of sensitive uses</i>

**Character Area 1**  
Urban streetscapes with increased housing density featuring attached dwellings, apartments and small lot detached dwellings

**Character Area 2**  
Diversity of attached and detached dwellings in well landscaped streets, integrated with townhouse and medium density dwellings

**Character Area 3**  
Spacious & leafy streetscapes with larger lots and predominantly detached dwellings, with some townhouse and apartment development in key locations

### 3.0 IMPLEMENTATION

#### 3.1 IMAGE, CHARACTER AND HOUSING

REQUIREMENTS

IMAGE & CHARACTER	
<b>R1</b>	Open space must be separated from development by a road interface unless otherwise agreed in writing by the responsible authority.
<b>R2</b>	The existing topography and indigenous vegetation (particularly River Red Gums) within Black Swamp must be protected as a significant area of local landscape character.
<b>R3</b>	Indigenous tree species must be used where a street adjoins the Lollypop Creek corridor (including Black Swamp).
<b>R4</b>	Street tree planting on future declared arterial roads must be established in accordance with the VicRoads Clear Zone Guidelines.
<b>R5</b>	Street trees must be provided on both sides of connector and local access roads, at intervals of no less than 15 metres unless otherwise agreed in writing by the responsible authority.
<b>R6</b>	A use or development that contains a bedroom and is located on land that is reasonably likely, in the opinion of the responsible authority, to be impacted by noise from the train operations in the RRL corridor / public transport reservation must be designed to ensure that internal noise levels in bedrooms is less than 65 dB L <sub>Amax</sub> and 40 dB L <sub>eq9h</sub> (night).*
<b>R7</b>	The Ison Road bridge and Bulban Rd crossing abutments and batters must be landscaped to the satisfaction of the responsible authorities.
<b>R8</b>	The Ison Road bridge and Lollypop Creek crossing abutments must be landscaped to the satisfaction of the responsible authorities.

\* This requirement does not apply where a Design and Development Overlay Schedule (DDO) is included in the Wyndham Planning Scheme to manage noise effects from train operations on the Regional Rail Link and the DDO is applied to relevant land in this Precinct.

#### HOUSING DENSITY EXAMPLES

##### Character Area 1

Urban streetscapes with increased housing density, featuring attached dwellings, apartments, and small lot detached dwellings.

Lot Type	Typical Lot Area	Range (% Mix)
Attached (separate title)	135 – 250	50 – 70
Detached	250 – 350	15 – 20
Detached	350 – 450	5 - 10
Detached	450 – 550	<5
Detached	550 – 700	0
Detached	>700	0
Group Housing (community title)	Varies	<5

GUIDELINES

IMAGE & CHARACTER	
<b>G1</b>	Significant elements of the landscape including trees, water bodies and built form such as public buildings and bridges should be used as focal points in the design of the new community.
<b>G2</b>	A gateway treatment, consisting of either built form, landscaping or both, should be provided at the future crossing of Ison Road and the Melbourne-Geelong Railway and beyond to its junction with Bulban Road.
<b>G3</b>	Street design and subdivision layout should provide for various access options for dwellings which front arterial roads (primary, secondary and local arterials). This includes internal street network access, rear loaded lots and service roads where appropriate.
<b>G4</b>	Street layout should maximise connections and views to key destination points such as the LTC and the Black Swamp and central water body.
<b>G5</b>	Housing along the Black Swamp and central water body should be sited responsively and take account of the topography and environmental conditions of these assets.
<b>G6</b>	Front fences abutting open space areas and waterway corridors should be visually permeable.
<b>G7</b>	Corner lots should address both streets with low fences up to a minimum of five (5) metres behind the front building line.
<b>G8</b>	A road or footpath should be provided to dwellings abutting open space or waterway corridors. All dwellings should have positive address to the open space and waterway corridors.
<b>G9</b>	The selection of street trees and landscaping should reinforce the movement hierarchy and the character across the precinct and be consistent with the Wyndham Street Tree Planting strategy.
<b>G10</b>	Street trees should form continuous canopies, frame view corridors, and provide strong avenues and shade to streetscapes.
<b>G11</b>	Sites in prominent locations such as within and approaching the LTC and on corners intersecting with key arterial and connector roads of the precinct should be developed to respond to their strategic location and preferably have greater height and density.
<b>G12</b>	Dwelling design should add to the precinct character by providing an attractive street address which encourages passive surveillance of public areas.
<b>G13</b>	Where Plan 2 shows residential land abutting the RRL corridor, consider provision for non-sensitive land use along the RRL corridor.
<b>G14</b>	Street tree planting should be in accordance with guidelines within Appendix 4.4.

##### Character Area 2

Diversity of attached and detached dwellings in well landscaped streets, integrated with townhouse and medium density housing dwellings.

Lot Type	Typical Lot Area	Range (% Mix)
Attached (separate title)	135 – 250	5 - 10
Detached	250 – 350	15 - 25
Detached	350 – 450	30 - 45
Detached	450 – 550	15 - 25
Detached	550 – 700	5 - 10
Detached	>700	<5
Group Housing (community title)	varies	<5

REQUIREMENTS

HOUSING	
<b>R9</b>	Achieve a minimum of 16 dwellings per NDha across the entire Precinct.  Lots must front or side: <ul style="list-style-type: none"> <li>Water bodies and lineal waterway corridors</li> <li>RRL reserve</li> <li>Metro Spur reserve</li> <li>Road network</li> <li>Open space</li> </ul>
<b>R10</b>	unless otherwise agreed to the satisfaction of the responsible authority
<b>R11</b>	Residential development across the precinct must include a full range of dwelling densities and housing types as outlined in the PSP Guidelines.
<b>R12</b>	To ensure medium and higher density residential development is prioritised in locations close to public transport, town centres and areas of high amenity.
<b>R13</b>	Multi-story buildings adjacent to waterways must not adversely overshadow vegetated waterway corridor areas.
<b>R14</b>	Dwellings with abuttal to the RRL must front a road unless otherwise agreed by the responsible authority.

GUIDELINES

HOUSING	
<b>G15</b>	A range of dwelling densities provided throughout residential areas to cater for a range of household types and provide built form diversity and interest in streetscapes.
<b>G16</b>	Medium-High density housing should be provided at a minimum density of 28 dwellings for NDHa and should be concentrated in areas of high amenity or convenience, including around the LTC, public open spaces (active and passive) and along public transport routes.

##### Character Area 3

Spacious and leafy streetscapes with larger lots and predominantly detached dwellings, with some townhouse and apartment development in key locations.

Lot Type	Typical Lot Area	Range (% Mix)
Attached (separate title)	135 – 250	< 5
Detached	250 – 350	5 - 10
Detached	350 – 450	20 - 35
Detached	450 – 550	20 - 35
Detached	550 – 700	20 - 35
Detached	>700	5 - 10
Group Housing (community title)	varies	<5

PSP 42.1

Future PSP

Quarry Site

Future PSP

Future PSP

PSP 43

	Precinct Structure Plan Area
	Adjoining Future PSP Area
	Arterial Road Existing
	Primary Arterial Road - 6 Lanes
	Secondary Arterial Road - 4 Lanes
	Boulevard Connector
	Connector Street
	Key Access Street
	Bridge
	Key Gateway
	Railway Line
	Proposed Railway Station
	Regional Rail Link (RRL)
	Railway Level Crossing
	Local Town Centre
	Mixed Use / Commercial
	Employment
	Mixed Use
	Community Facilities
	Government Education Facility
	Non-Government Education Facility
	Residential
	Active Open Space
	Passive Open Space
	400m Pedshed
	Encumbered Open Space
	Encumbered Land for Conservation
	Interim Fire Threat Edge
	Trees to be Retained
	Existing Urban Area
	Future Urban Area
	Quarry Buffer

Plan 5

Scale: 1:12,500 @ A3 Date: April 2013  
 0 100 200 400m

Open Space, Natural Systems & Community Facilities

### 3.2 OPEN SPACE & NATURAL SYSTEMS, BUSHFIRE MANAGEMENT AND COMMUNITY FACILITIES

#### REQUIREMENTS

OPEN SPACE	
<b>R15</b>	All public landscaped areas must be designed to a high standard that is attractive, accessible and easy to maintain.
<b>R16</b>	Appropriately scaled lighting (utilising baffled fittings to reduce light spill) must be installed along all major pedestrian thoroughfares traversing public open space and shared paths.
<b>R17</b>	The total provision of open space for each land parcel must be consistent with the open space requirements in Table 3 unless otherwise agreed in writing by the responsible authority.
<b>R18</b>	Development abutting open space must be designed to provide passive surveillance, through the appropriate siting of windows, balconies and pedestrian access points.
<b>R19</b>	Fencing of parkland must be low scale and/or permeable to facilitate public safety and surveillance.
<b>R20</b>	Shared trails within waterway reserves must be designed to enable vehicle access for maintenance purposes to the satisfaction of Melbourne Water.
<b>R21</b>	Maintenance access to waterways and drainage infrastructure is to be provided and delivered to Melbourne Water's requirements.

#### GUIDELINES

OPEN SPACE	
<b>G17</b>	Active recreation reserves should be designed to maximise co-location opportunities between complementary sports and adjoining school facilities.
<b>G18</b>	The design and layout of open space should maximise water use efficiency, storm water quality and long term viability of vegetation through the use of Water Sensitive Urban Design (WSUD) initiatives.
<b>G19</b>	Passive parks should cater for a broad range of users and support both structured and informal recreation activities.
<b>G20</b>	Passive parks within and adjacent to the Black Swamp should provide clear delineation of areas of conservation significance.
<b>G21</b>	Advice should be sought from qualified Council staff regarding suitability of proposed tree species prior to confirming planting schedule.
<b>G22</b>	Subdivision design should provide pedestrian and cyclist crossings of waterway corridors generally at a minimum interval of 400m.
<b>G23</b>	The design and layout of waterways, wetlands, water bodies and open space adjoining /integrated with waterways, wetlands and water bodies should be in accordance with Appendix 4.1 Water Design Guidelines.

#### REQUIREMENTS

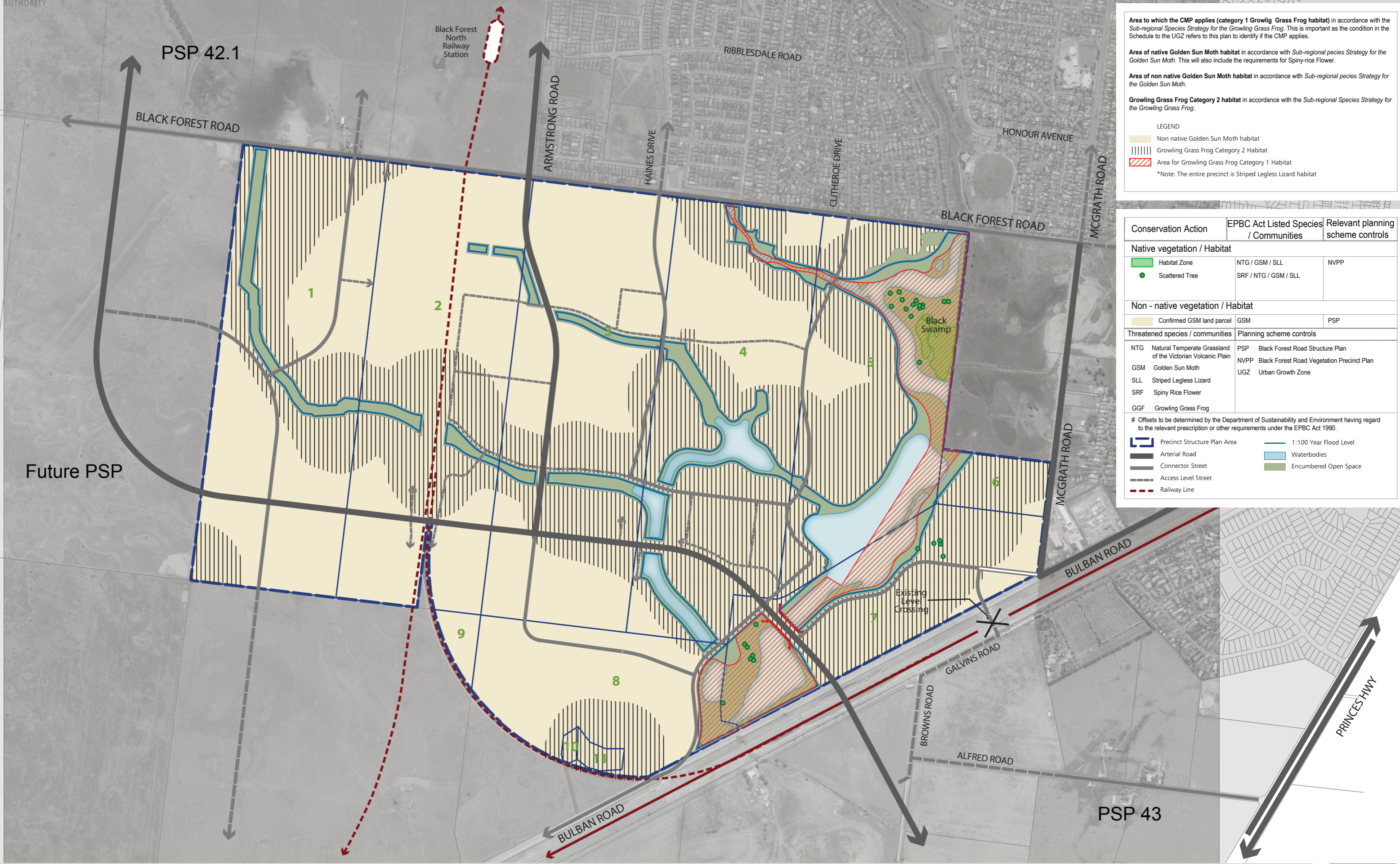
BIODIVERSITY AND NATURAL SYSTEMS	
<b>R22</b>	The layout and design of the waterway corridors including the design of paths, bridges and boardwalks must respect biodiversity and natural systems, and ensure appropriate water quality through best practice stormwater treatment and use of Water Sensitive Urban Design to the satisfaction of responsible authorities.
<b>R23</b>	Landscape master planning of open space adjacent to the Black Swamp corridor must be designed to protect areas of environmental and cultural significance (i.e habitat zones) from pedestrian and vehicle access.

#### GUIDELINES

BIODIVERSITY AND NATURAL SYSTEMS	
<b>G24</b>	Landscaping adjacent to habitat zones identified to be retained should be complementary to the natural environment, particularly along the Black Swamp and waterway corridors.
<b>G25</b>	The layout and design of waterway corridors and stormwater systems should demonstrate how retained vegetation and any designated conservation areas are protected from adverse stormwater impacts.

#### REQUIREMENTS

OPEN SPACE CONTRIBUTIONS	
	Further to the public open space contribution required at Clause 52.01 of the Wyndham Planning Scheme, this provision sets out the amount of land to be contributed by each property in the precinct and consequently where a cash contribution is required in lieu of land.
	All land owners must provide a public open space contribution equal to 3% of Net Developable Area(NDA) upon subdivision of land in accordance with the following:
<b>R24</b>	<ul style="list-style-type: none"> <li>Where land is required for unencumbered open space purposes as shown in Plan 5 and specified in Table 1 and is less or equal to 3% of NDA that land is to be transferred to Council at no cost;</li> <li>Where no land or less than 3% of NDA is shown in Plan 5 and specified in Table 1, a cash contribution is to be made to Council to bring total open space contribution to a value equal to 3% of NDA. The cash contribution is to be made at a rate as per outlined in the Wyndham West DCP per NDHa;</li> <li>Where land required for unencumbered open space purposes as shown in Plan 5 and specified in Table 1 is more than 3% of NDA, Council will pay an amount equivalent to the value of the additional land being provided by that property. (with CPI)</li> </ul>
	Note, refer also Property Specific Land Budget Table 3 for detailed individual property open space obligations and percentages.
<b>R25</b>	To facilitate the transfer of active open space land (8ha to the west of RRL and south of Ison Road) that is not provided for in the Wyndham West DCP, a Section 173 Agreement (under the P&E Act 1987) or alternative mechanism must be prepared and executed by the land owner(s) with Wyndham City prior to the development of any land (by the particular landowner) located west of the Regional Rail Link and within 400 metres of the Active Open Space reserve to ensure the transfer of this land. The agreement must be registered on the title to the land.



**Area to which the CMP applies (category 1 Growling Grass Frog habitat)** in accordance with the Sub-regional Species Strategy for the Growing Grass Frog. This is important as the condition in the Schedule to the UGZ refers to this plan to identify if the CMP applies.

**Area of native Golden Sun Moth habitat** in accordance with Sub-regional species Strategy for the Golden Sun Moth. This will also include the requirements for Spiny-rice Flower.

**Area of non native Golden Sun Moth habitat** in accordance with Sub-regional species Strategy for the Golden Sun Moth.

**Growling Grass Frog Category 2 habitat** in accordance with the Sub-regional Species Strategy for the Growing Grass Frog.

**LEGEND**

- Non native Golden Sun Moth habitat
- Growling Grass Frog Category 2 Habitat
- Area for Growling Grass Frog Category 1 Habitat

\*Note: The entire precinct is Striped Legless Lizard habitat

Conservation Action	EPBC Act Listed Species / Communities	Relevant planning scheme controls
<b>Native vegetation / Habitat</b>		
Habitat Zone	NTG / GSM / SLL	NVPP
Scattered Tree	SRF / NTG / GSM / SLL	
<b>Non - native vegetation / Habitat</b>		
Confirmed GSM land parcel	GSM	PSP
<b>Threatened species / communities</b>		
NTG Natural Temperate Grassland of the Victorian Volcanic Plain	PSP	Black Forest Road Structure Plan
GSM Golden Sun Moth	NVPP	Black Forest Road Vegetation Precinct Plan
SLL Striped Legless Lizard	UGZ	Urban Growth Zone
SRF Spiny Rice Flower		
GGF Growling Grass Frog		
# Offsets to be determined by the Department of Sustainability and Environment having regard to the relevant prescription or other requirements under the EPBC Act 1990.		
Precinct Structure Plan Area	1:100 Year Flood Level	
Arterial Road	Waterbodies	
Connector Street	Encumbered Open Space	
Access Level Street		
Railway Line		



REQUIREMENTS

**BUSHFIRE MANAGEMENT**

**R26** Unless a subdivision meets the standards set out in either G26 or G27, it must provide defendable space between a fire threat identified on the Open Space, Natural Systems and Community Facilities Plan in this precinct structure plan and a dwelling to the satisfaction of the CFA.

Where a lot contains defendable space the following applies:

Before the statement of compliance is issued under the Subdivision Act 1988 the owner must enter into an agreement with the responsible authority under Section 173 of the Planning and Environment Act 1987 and make application to the Registrar of Titles to have the agreement registered on the title to the land under Section 181 of the Act. The agreement must set out the following matters:

- A building envelope and that a building must not be constructed outside of the building envelope.
- That a building must not be constructed on the lot unless it is constructed to 12.5 BAL.
- The area of defendable space applicable to the lot with the following restrictions on vegetation during a declared fire danger period:
  - Within 10 metres of a building, flammable objects (such as plants, mulches and fences) must not be located close to the vulnerable parts of the building (such as windows, decks and eaves).
  - Grass must be no more than five centimetres in height.
  - Trees must not overhang or touch any part of a building.
  - Leaves and vegetation debris must be removed at regular intervals.
  - Shrubs must not be planted under trees.
  - Plants greater than ten centimetres in height at maturity must not be placed directly in front of a window or other glass feature.
  - A tree canopy must not be closer than two metres to another tree canopy.
  - Total tree canopies must cover no more than 15% of the area of the lot at maturity.

This does not apply where the Country Fire Authority states in writing that a Section 173 agreement is not required for the subdivision or lot.

**R28** Provide a road network that enables at least two safe egress routes away from the fire hazard.

For the purposes of Clause 56.06-7, the requirements of the relevant fire authority are, unless otherwise approved by the CFA:

- Constructed roads must be a minimum of 7.3m trafficable width where cars park on both sides, or:
  - A minimum of 5.4m in trafficable width where cars may park on one side only.
  - A minimum of 3.5m width with no parking and 0.5m clearance to structures on either side, and if this width applies, there must be passing bays at least 20m long, 6m wide, and located not more than 200m apart.
- R29**
- Roads must be constructed so that they are capable of accommodating a vehicle of 15 tonnes for the trafficable road width.
  - The average grade of a road must be no more than 1 in 7 (14.4% or 8.1°).
  - The steepest grade on a road must be no more than 1 in 5 (20% or 11.3°) with this grade continuing for no more than 50 metres at any one point.
  - Dips in a road must have no more than a 1 in 8 grade (12.5% or 7.1°) entry and exit angle.

Constructed dead end roads more than 60 m in length from the nearest intersection must have a turning circle with a minimum radius of 8 m (including roll-over curbs if they are provided).

**R30** Planting in streets and public spaces within defendable space must be designed to take into account impact on fire risk.

Before the commencement of works for a stage of subdivision a Construction Management Plan (CMP) that addresses Bushfire Risk Management must be submitted to and approved by the responsible authority and the CFA. The CMP must specify, amongst other things:

**R31** Measures to reduce the risk from fire within the surrounding rural landscape and protect residents from the threat of fire

A separation buffer, consistent with the separation distances specified in AS3596-2009, between the edge of development and non-urban areas.

How adequate opportunities for access and egress will be provided for early residents, construction workers and emergency vehicles.

REQUIREMENTS

**BUSHFIRE MANAGEMENT**

A Construction or Engineering Plan required under a subdivision permit must show:

- The location of static water supplies for fire fighting purposes that are:
- Accessible to fire fighting vehicles
- Have sufficient volume to support effective fire fighting

or

**R32**

- Strategically positioned fire hydrants installed on the potable water supply system in addition to the fire hydrants installed on the recycled water supply system (where present).

and

- Water supply design, connections and flow rates.

All to the satisfaction of the CFA.

GUIDELINES

**BUSHFIRE MANAGEMENT**

Where a lot capable of accommodating a dwelling is proposed up slope or on flat land and adjacent to a fire threat identified on the Open Space, Natural Systems and Community Facilitation Plan in this precinct structure plan, a plan of subdivision must provide for defendable space in the form of a 19 metre wide road reserve between the edge of the fire threat and the lot on which a dwelling may be developed.

**G26**

A restriction on a plan of subdivision registered under the Subdivision Act 1988 must specify that a dwelling constructed on land shown as 'Bushfire Prone Area' on the Open Space, Natural Systems and Community Facilitation Plan in this precinct structure plan must not be constructed to a standard less than BAL 12.5.

**G27**

Where a lot capable of accommodating a dwelling is proposed down slope and adjacent to a fire threat identified on the Open Space, Natural Systems and Community Facilitation Plan in this precinct structure plan, a plan of subdivision must provide for defendable space comprising a road reserve of at least 19 metres width between the edge of the fire threat and the lot on which a dwelling may be developed plus the additional width of defendable space specified below. The additional defendable space may be on public or private land:

Down slope (degrees)	additional defendable space (metres)	Total defendable space
>0-5	3	22
>5-10	6	25
>10-15	9	28
>15-20	13	32
>20	to the satisfaction of the relevant fire authority.	

**G28**

Where defendable space is proposed on a lot capable of accommodating a dwelling requirement R31 in this precinct structure plan applies.

REQUIREMENTS

**COMMUNITY FACILITIES AND EDUCATION**

**R33** Street layout should provide 3 sides as per DEECD requirements.

**R34** Where the responsible authority is satisfied that land shown as a non-government school site is unlikely to be used for a non-government school, that land may be used for an alternative purpose with is generally in accordance with Precinct Structure Plan and consistent with the provisions on the applied zone.

**COMMUNITY FACILITIES AND EDUCATION**

**G29** Community facilities should provide a focal point for community activity and interaction within each neighbourhood.

**G30** Community facilities should be planned and designed for flexible use.

**G31** Community facilities should be located within or adjacent to the LTC.

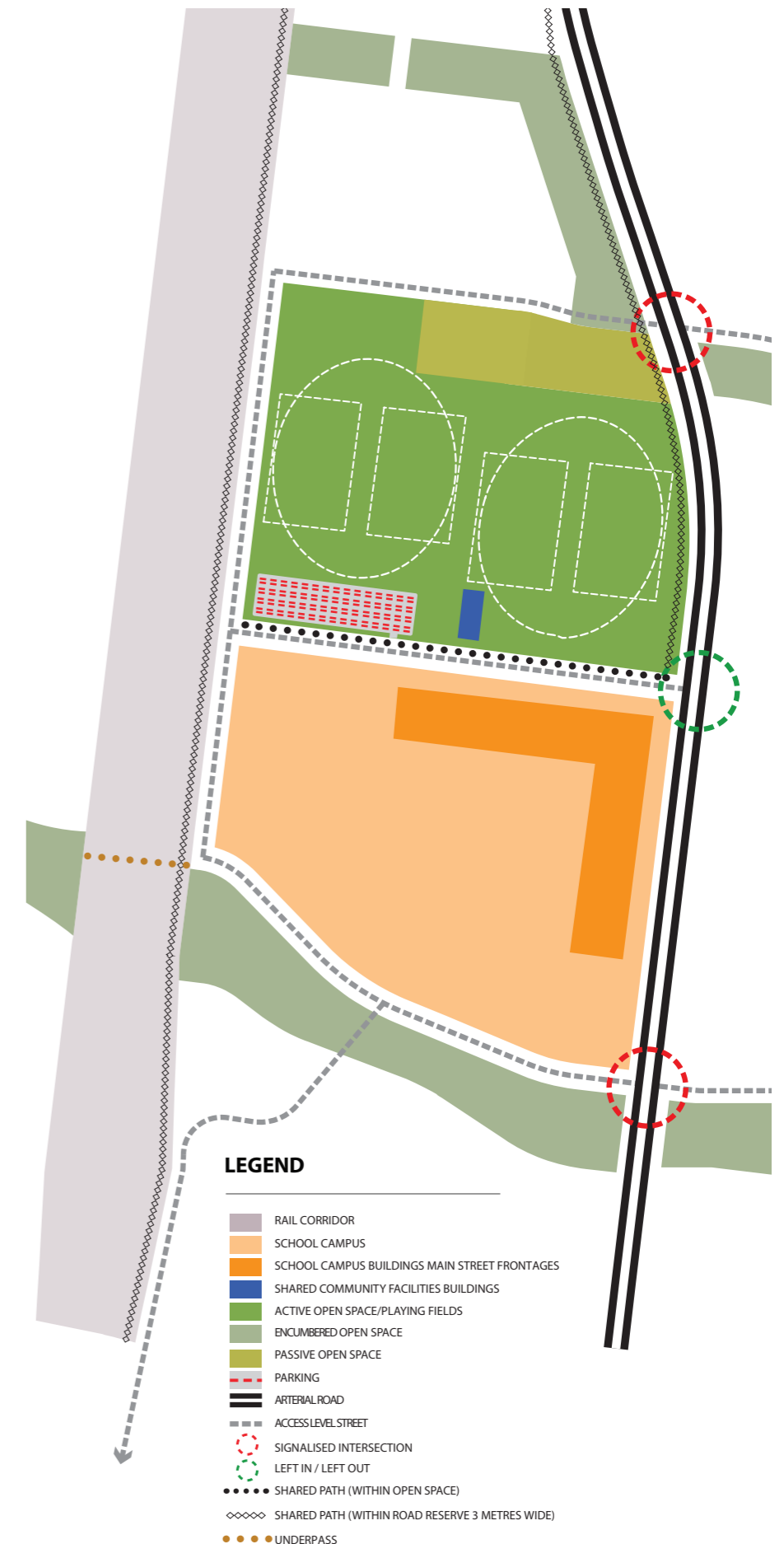
**G32** Streets layout should provide three road frontages for school sites.

**G33** Consideration should be given for community facilities having green infrastructure (i.e green roofs, reflective paint, rainwater tanks for toilets and landscape watering and bicycle facilities)

OPEN SPACE DELIVERY GUIDE					
PARK ID	AREA (HA)	PARK TYPE	DESCRIPTION	LOCATION	RESPONSIBILITY
OS0	1ha passive open space adjoining encumbered waterway corridor	Social / Family Recreation	As the primary open space focal point in the area, this park provides opportunities for more intensive recreation infrastructure. This could include a larger multi-age playground combined with social / family recreation with destination facilities (including drinking fountains, native trees and artificial structural shade from daytime sun).	Strategically located central to the residential development west of the future railway line, this park will be in walking distance for all residents in the western precinct.	Wyndham CC
OS1	0.97ha passive open space adjoining encumbered waterway corridor	Social / Family Recreation	As the primary open space focal point in the area, this park provides opportunities for more intensive recreation infrastructure. This could include a larger multi-age playground combined with social / family recreation with destination facilities (including drinking fountains, native trees and artificial structural shade from daytime sun).	Strategically located central to the residential development west of the future railway line, south of the east west connector this park will be in walking distance for all residents in the western precinct.	Wyndham CC
OS2	1ha passive open space	Social / Family Recreation	The open space will provide a larger multi-age playground combined with social family recreation with destination facilities (including drinking fountains, native trees and artificial structural shade from daytime sun).	This park is located centrally within in the residential catchment bounded by Ison Road the Regional Rail link spur and the southern portion of the waterways. 'Green' road links will encourage the visual connection between these locations.	Wyndham CC
OS3	.42 ha passive open space + 1.27ha encumbered drainage reserve	Nature Conservation/ Contemplative	The open space will present opportunities to take advantage of views and vistas back to OS3 and across the ~400x400m water body and lends itself to a more reflective and environmentally sensitive social & family recreation functions (including drinking fountains, native trees and daytime sun).	This park is located on the western banks of the major water body. At this location the open space will connect into the remaining remnant portion of Lollypop creek. This portion of creek retains some of the only remaining native vegetation on site. EVC 653 Aquatic Herb land). The location of the open space will generally service the southern part of the precinct, bounded by the RRL and Ison Road.	Wyndham CC
OS4	.98 ha passive open space + 0.40 encumbered waterway corridor	Civic Gathering	This space presents the opportunity for an iconic, regional social / family destination place. A location for public gathering and an event space (including hard paved areas, drinking fountains, native trees and artificial structural shade from daytime sun).	This park is anchored off the Town Centre main street 'promenade'. It is a critical piece of space that will integrate the town centre and the community with the waterways. The open space is connected to an internal hike & bike system of 10km+ around the water body, which will link into the regional Lollypop Creek trail.	Wyndham CC
OS5	0.8ha passive open space + 0.22ha encumbered waterway corridor	Social / Family Recreation	The open space is positioned to retain 5 existing River Red Gums. It is also strategically located along the shared trail network, which will help connect these residents to the Town Centre & community hub. This park provides opportunities for more intensive recreation infrastructure. This could include a larger multi-age playground combined with social / family recreation with destination facilities (including drinking fountains, native trees from daytime sun).	This open space will provide amenity to the eastern precinct of the PSP, bounded by the wetland body, the railway and McGrath Road.	Wyndham CC
OS6	0.5ha passive open space + 2.8ha encumbered waterway corridor	Play / Natural	This smaller park would suit simple design, planting and infrastructure to support low key and contemplative recreation functions to take advantage of the amenity and vistas created by the adjoins. It is also well integrated with the extensive waterway and shared path network in this part of the precinct (including drinking fountains, native trees from daytime sun).		Wyndham CC
OS7	1.7ha passive open space + 1.8ha encumbered waterway corridor	Educational/ Contemplative/ Play	This open space is proposed to preserve an area of cultural significance. Planting and design will reflect the indigenous landscape. Open Space facilities will be sympathetic to the landscape and will support educational and contemplative recreation functions, with potential boardwalk connections to the black swamp.  Key aspects of park design include: <ul style="list-style-type: none"> <li>• 2.5m shared trail on both sides</li> <li>• Retention of significant River Red Gums and biodiversity attributes</li> </ul>	Between Black Forest Road and Black Swamp areas next to the eastern water body.	Wyndham CC
OS8	.4ha passive open space + 0.41 encumbered waterway corridor	Play / Natural	This park provides opportunities for moderate recreation infrastructure. This could include a larger multi-age playground combined with social / family recreation with destination facilities (including drinking fountains, native trees and artificial structural shade from daytime sun).	South of the Black Swamp next to the eastern water bodies.	Wyndham CC
OS9	0.4ha passive open space + 0.1ha encumbered waterway corridor	Contemplative	This smaller park would suite simple design, planting and infrastructure to support low key and contemplative recreation functions to take advantage of the amenity and vistas created by the large-scale wetlands and waterways it adjoins, (including drinking fountains, native trees and artificial structural shade from daytime sun). Possible site for a community garden.	North of the northern main water bodies.	Wyndham CC
OS10	0.62 ha passive open space + 0.1ha encumbered waterway corridor	Visual Amenity	Located 400m from the local town centre, this park lends itself to fantastic vistas across the water bodies. It is also well integrated with the extensive waterway and shared path network in this part of the precinct.	West of the eastern water bodies.	Wyndham CC
OS11	1.05 passive open space	Social / Family Recreation	Should provide play and shelter facilities, good potential location for a skate park/ youth activity (including drinking fountains, native trees and artificial structural shade from daytime sun).	This park will add value and supporting amenity to the active recreation in this location. It is located adjacent to the Active Open Space, A1, co-located with the secondary school.	Wyndham CC
OS12	1ha passive open space	Play / Natural	This park lends itself to including facilities for youths and families with older children. It is also well integrated shared path network in this part of the precinct (including drinking fountains, native trees and artificial structural shade from daytime sun)	Located 400m of the secondary school and Active Open Space A1	Wyndham CC

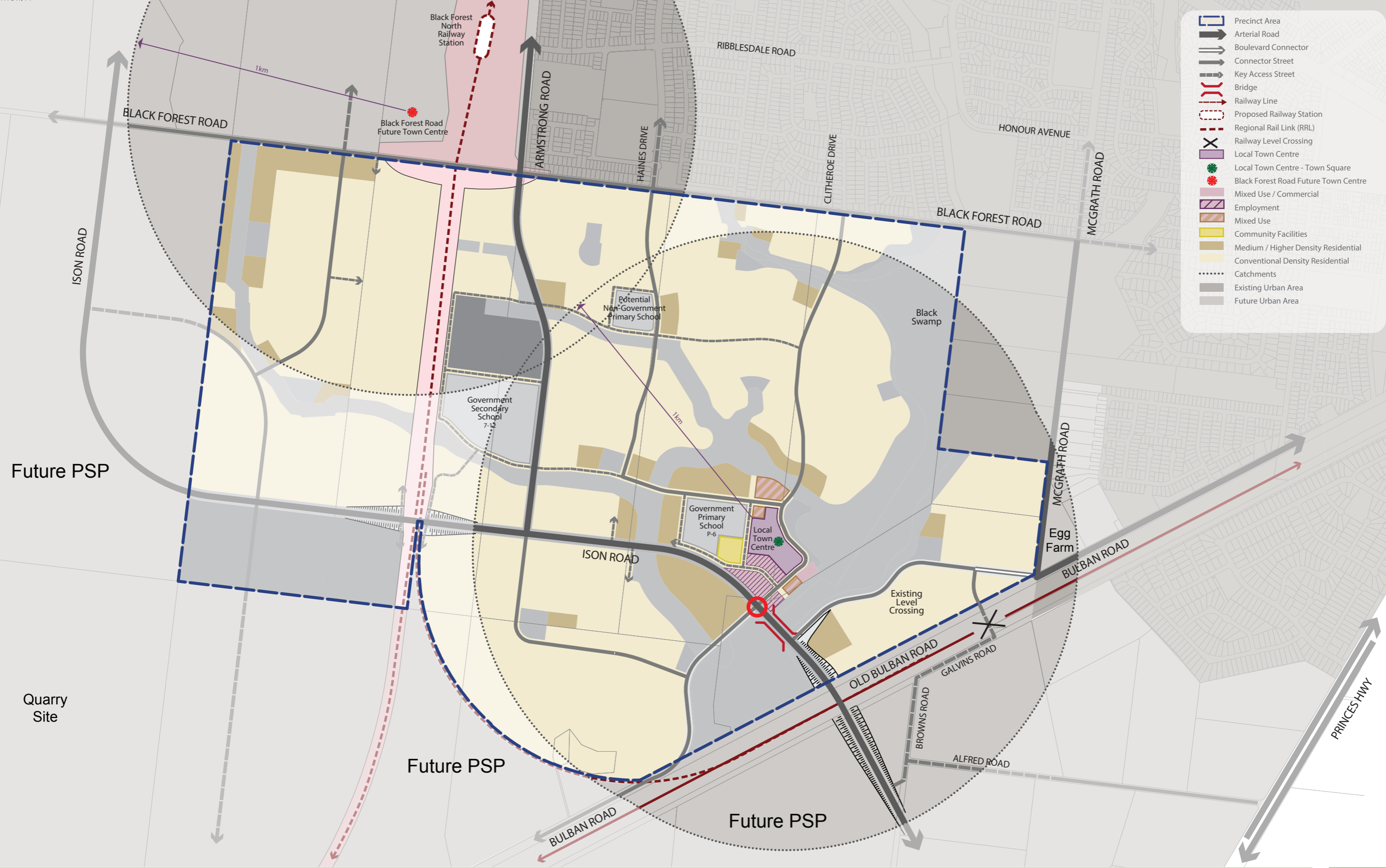
OPEN SPACE INVENTORY					
PARK ID	AREA (HA)	PARK TYPE	DESCRIPTION	LOCATION	RESPONSIBILITY
<b>Linear Parks</b>	18.1ha encumbered waterway corridors	Access /trails	These linear parks offer the opportunity for passive recreation through shared paths, seating, shade and nodal destination/rest points.	Essential to pedestrian connectivity throughout the development.	Melbourne Water/ Wyndham CC
<b>A1</b>	7.59ha Active open Space	Active	<p>Active Open space reserve, the primary purpose of which is to provide cricket and AFL club based sport. It should also include passive elements such as walking paths, seats and trees as well as playground facilities.</p> <p>Incorporates:</p> <ul style="list-style-type: none"> <li>2 x senior AFL / cricket ovals, cricket nets</li> <li>district playground</li> <li>possible site for a community garden</li> <li>160 x car parking spaces</li> <li>1 oval pavilion</li> <li>(including drinking fountains, native trees and artificial structural shade from daytime sun)</li> </ul>	This Active recreational area west of Armstrong Road will add value and supporting amenity co-located with the Catholic Primary School and Secondary School.	Wyndham CC
<b>A2</b>	23.55ha Active open Space	Active	<p>8.0 hectares provided "gratis" to Council and not compensated in DCP.</p> <p>This is a larger active reserve, the primary purpose of which is to provide cricket, AFL and soccer club based sport. It should also include passive elements such as walking paths, seats and trees playground facilities.</p> <p>Incorporates:</p> <ul style="list-style-type: none"> <li>2 x senior AFL / cricket ovals</li> <li>cricket nets</li> <li>3 x soccer pitches</li> <li>12 x tennis courts</li> <li>8 x softball Fields</li> <li>4 x netball courts</li> <li>1250sqm district playground</li> <li>240 x car parking spaces</li> <li>1 oval pavilion</li> <li>1 soccer pavilion</li> <li>1 tennis pavilion</li> <li>1 netball pavilion</li> <li>1 softball pavilion</li> <li>1 netball pavilion</li> <li>(including drinking fountains, native trees and artificial structural shade from daytime sun)</li> </ul>	South west corner of the precinct bounded by the RRL and South of Ison Road.	Wyndham CC
<b>A3</b>	14.61ha Water Bodies	Water Based Recreation	Water based regional activity area. Sports clubs and schools to be associated with the water body uses - rowing, kayaking, canoeing, fishing, dragon boat racing, sailing.	East of the LTC. Ison Road	Wyndham CC (Melbourne Water – water quality only)*

Figure 1: A1 Community Hub



\*The potential for an active recreation water body situated offline from Lollypop Creek is to be explored. If any active recreational water bodies are proposed, a formal ownership and maintenance agreement prepared between the developer and an appropriate future public manager such as Council or Parks Victoria must be prepared and submitted with any application for subdivision. Any active water body proposed must maintain flood conveyance and storage functions as required by Melbourne Water, and must demonstrate that it meets applicable EPA water quality requirements, and any maintenance requirements of the designated future public manager.

	Precinct Area
	Arterial Road
	Boulevard Connector
	Connector Street
	Key Access Street
	Bridge
	Railway Line
	Proposed Railway Station
	Regional Rail Link (RRL)
	Railway Level Crossing
	Local Town Centre
	Local Town Centre - Town Square
	Black Forest Road Future Town Centre
	Mixed Use / Commercial
	Employment
	Mixed Use
	Community Facilities
	Medium / Higher Density Residential
	Conventional Density Residential
	Catchments
	Existing Urban Area
	Future Urban Area



**Plan 7**

Scale: 1:12,500 @ A3 Date: April 2013  
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### 3.3 EMPLOYMENT AND TOWN CENTRES

#### Local Town Centre (LTC)

The Local Town Centre (LTC) is located to the north of Ison Road, and adjacent (west) of the main water body. The LTC will be central to the proposed residential catchment of Black Forest Road South Precinct.

The LTC will be well connected to the residential catchment by Ison Road, the local town centre connector street and the pedestrian and cycle networks along the waterway and lineal corridors. A permeable road network and a series of nodal destinations and dedicated pedestrian and cycle paths will enhance connectivity both within the LTC and the wider region.

The location of the LTC capitalises on opportunities for exposure and passing trade by providing a frontage to Ison Road. Uses which benefit from exposure and passing traffic, such as commercial premises and mixed use developments will provide quality built form outcomes along Ison Road. The anchor sites on the corner of Ison Road and the LTC connector street will frame a clear entrance to the LTC. The southern approach to the LTC along the local town centre connector street will also be framed by high quality built form outcomes which may include high density housing overlooking of the northern water body, shop top offices, mixed use outcomes and urban public spaces.

The public open space node is located in north east corner of the LTC and forms a significant transition between the urban form of the LTC and the 'natural' environment of the water bodies. The public open space node is located on a peninsula which offers the opportunity for a consolidated stopping point within a regional pedestrian and cycle network and a regional destination with a strong connection to water. Potential uses of the public space node include an amphitheater or a community events space accommodating up to 4,000 people, regional play facilities, water play opportunities, recreation and future uses such as a cafe and/or restaurant or other community facilities.

The LTC's relationship to water has influenced its design and planning, giving a unique sense of character and place. The LTC will have a strong focus on both local convenience and employment opportunities. A variety of employment opportunities will be available and include the primary school, community centre, commercial spaces, food outlets, retail premises, mixed use outcomes and shop top offices. The LTC will accommodate employment sectors such as education, health, retail, hospitality, and small and medium size businesses. The LTC will provide employees access to convenient services including health, retail, entertainment and recreation.

Diverse housing outcomes including high and medium density housing types in the northern part of the Local Town Centre will have outlook onto a series of quality public spaces such as the northern wetlands, water bodies and parks and will take full advantage of solar orientation. Medium and higher density housing around the Local Town Centre will benefit from the amenities of the LTC whilst enhancing opportunities for passive surveillance of the LTC.

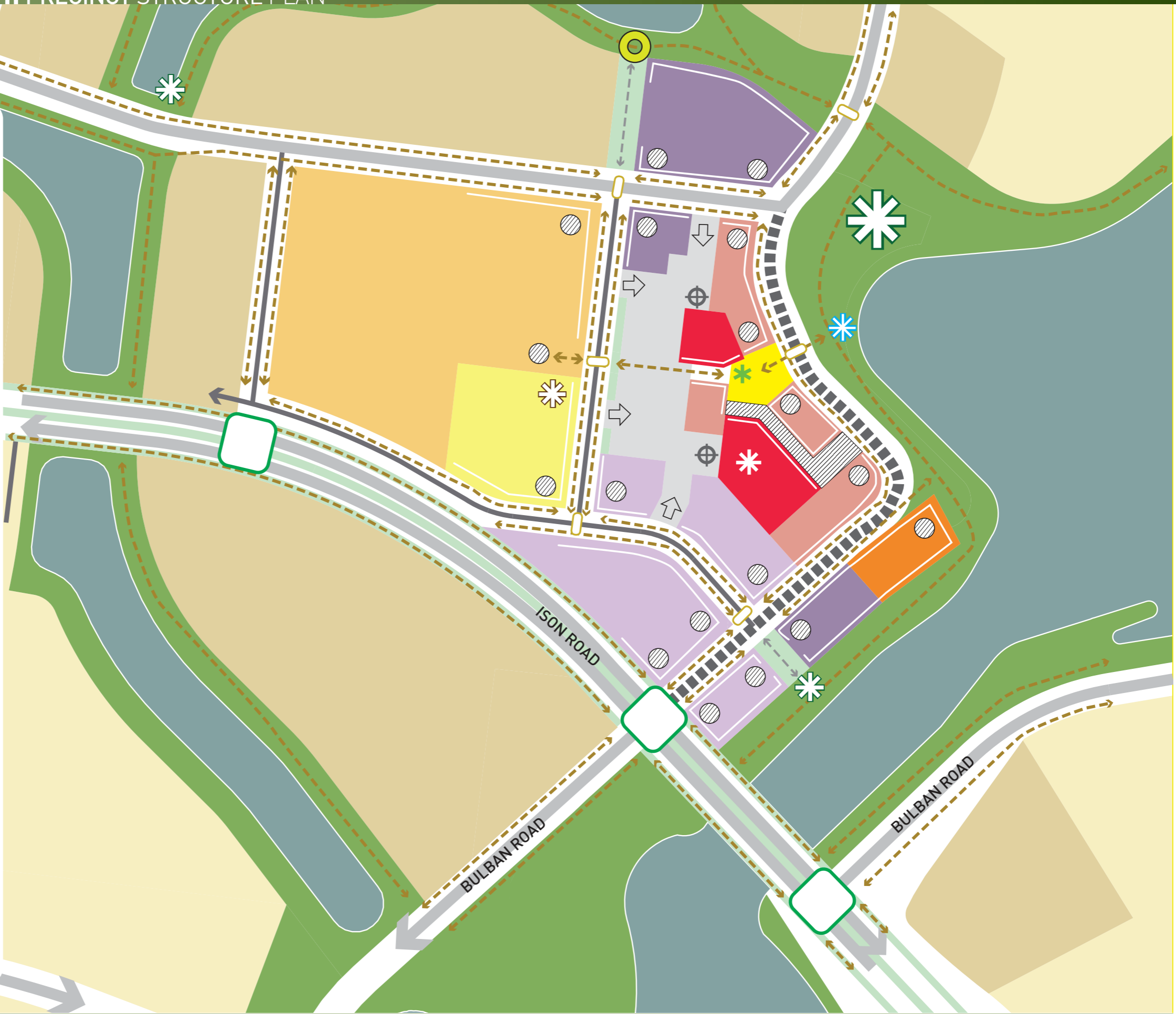
The LTC connector road is an integral part of the major public space node. The street will serve as a unique destination and will provide opportunities for uses such as retail, offices, medium density residential development, a boathouse/tavern, cafe's and restaurants and the major public space node to take advantage of the views across the water body to the east.

The LTC connector road design will lead pedestrians to the heart of the civic town square. The anchor retail, specialty retail and commercial uses within the LTC are interconnected with the town square to create a vibrant and active public space which acts as a meeting space for the surrounding community. The Town Square is the key nodal destination, part of the series of nodal destinations throughout the PSP area.

The design of the LTC has considered future retail, commercial and mixed use opportunities resulting in a staged LTC design. The LTC will provide services to the immediate catchment while retaining the possibility of expansion in the future. Expansion will be subject to market demands, future permit applications and the impact of expansion on the network of Local Town Centres within the region.

**LEGEND**

- ARTERIAL ROAD
- CONNECTOR ROAD
- LTC CONNECTOR ROAD
- ACCESS LEVEL 2 STREET
- ACCESS LEVEL 1 STREET
- LANEWAY / PEDESTRIAN LINK
- AT GRADE CARPARK
- KEY PEDESTRIAN LINK
- PEDESTRIAN CROSSING (treatment to be determined)
- PEDESTRIAN SIGNALISED CROSSING
- CAR PARK ACCESS
- SIGNIFICANT BUILT FORM
- LOADING DOCKS
- FRONTAGES TO ADDRESS ROAD OPEN SPACE
- PRIMARY RETAIL
- PRIMARY RETAIL ANCHOR
- RETAIL ON GROUND FLOOR
- COVERED PLAZA
- MIXED USE
- EMPLOYMENT / COMMERCIAL
- MIXED USE / COMMERCIAL
- TOWN SQUARE
- MEDIUM / HIGHER DENSITY RESIDENTIAL
- CONVENTIONAL RESIDENTIAL
- PRIMARY SCHOOL
- COMMUNITY FACILITIES
- EARLY LEARNING
- SIGNIFICANT OPEN SPACE NODE
- OPEN SPACE NODE
- CAFE / RESTAURANT
- FEATURE NODAL OPEN SPACE
- OPEN SPACE (various types)
- WATERBODY
- LANDSCAPING



REQUIREMENTS

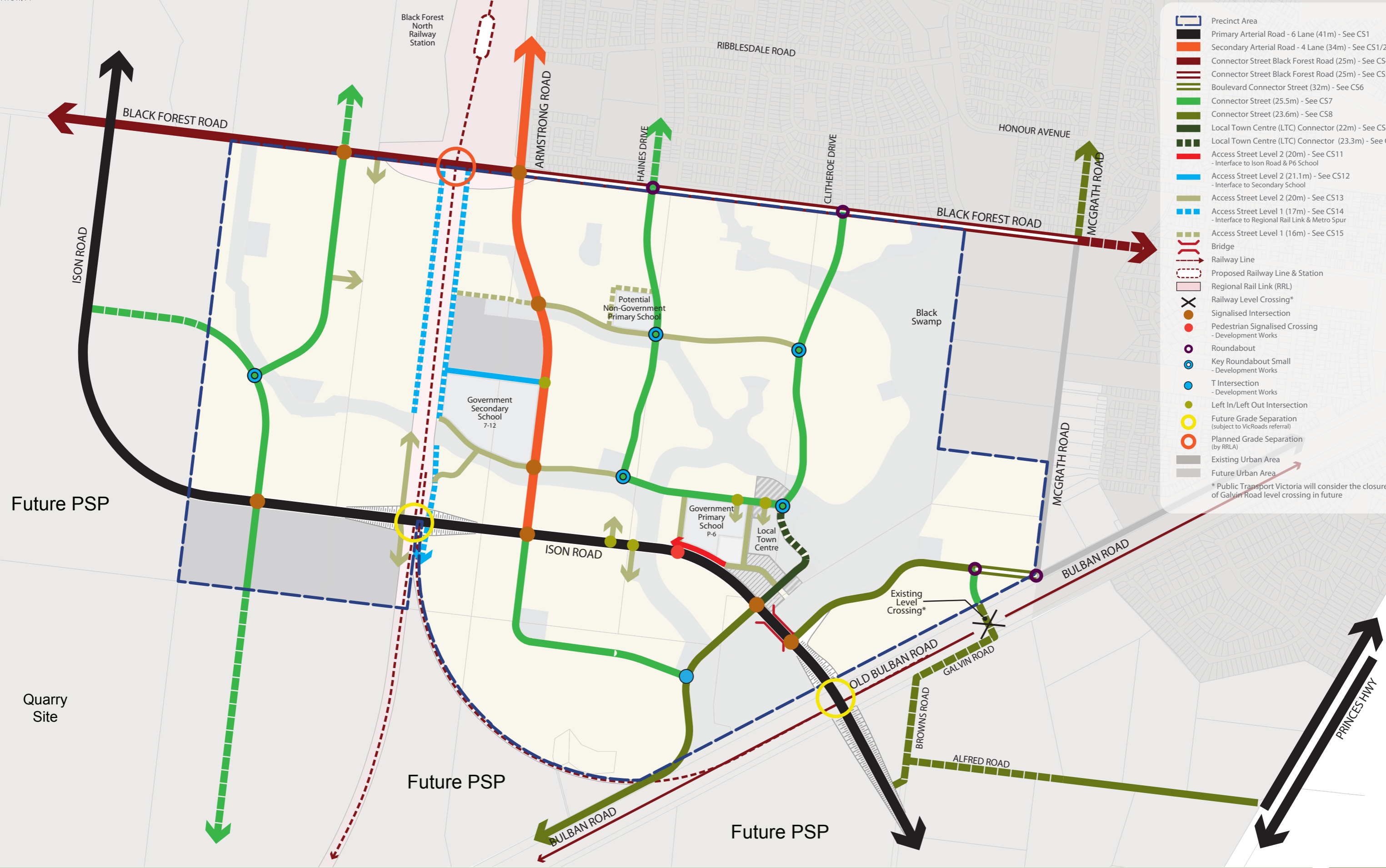
LOCAL TOWN CENTRE	
<b>R35</b>	The LTC design must be generally in accordance with the Local Town Centre concept plan shown in Plan 8.
<b>R36</b>	The design of the LTC must also respond to the Local Town Centre Design Guidelines in Appendix 4.3.
<b>R37</b>	Supermarkets and secondary anchors must have frontages that directly address the main street or town square so that the use integrates with and promotes activity within the main street.
<b>R38</b>	Local Town Centres must include provision for commercial uses including office.
<b>R39</b>	<p>The design of the Local Town Centre must address key view lines into and throughout the centre. Particular consideration needs to be given to key corners and the built form outcomes on these corners due to their prominence and role as the 'arrival' points into the Local Town Centre. These key 'gateway' locations include:</p> <ul style="list-style-type: none"> <li>• The intersection of Ison Road and the local town centre connector street;</li> <li>• The entrance to the Local Town Centre from the north along the local town centre connector street;</li> <li>• The entrance to the Local Town Centre from the west from Ison Road.</li> </ul>
<b>R40</b>	<p>The design of the Local Town Centre must address key view lines to and from water bodies and wetlands which surround the Local Town Centre. In addition, uses and built form outcomes from the northwest corner of the Local Town Centre adjacent to Ison Road must maximise views and orientate towards the water.</p> <p>Buildings which form part of the Local Town Centre must:</p>
<b>R41</b>	<ul style="list-style-type: none"> <li>• Provide primary access to tenancies from the local town centre connector road;</li> <li>• Be built to the street front. Where buildings are set back from the street front, the frontage of the building must be active and must be designed in a way which contributes to the public domain;</li> <li>• Include car parking and service infrastructure to the rear or side of the main street frontage.</li> </ul>
<b>R42</b>	Building facades on side streets (excluding shop fronts) and continuous walls must not exceed 10 metres without articulation, fenestration, activity or visual interest.
<b>R43</b>	Circulation and permeability throughout the Local Town Centre must ensure that the key destinations within the Local Town Centre are easily accessible by walking or cycling. In particular, east/west connections through the retail core between the water bodies and linear open space, town square, community and education facilities must be provided.
<b>R44</b>	Active and articulated frontages must be located to Ison Road, the local town centre connector road, the town square, the water bodies, wetlands and open space which surround the Local Town Centre.
<b>R45</b>	The town square (or similar) must have a strong relationship to the anchor retail, specialty retail and commercial uses as well as to the local town centre connector street and the open space located to the east of the town centre core. Pedestrian connections must be provided from the town square to the education and community facilities located to the west of the town centre core. The final configuration of the town square must consider passive surveillance opportunities, key pedestrian circulation and design outcomes which create an attractive destination and supports a range of uses.
<b>R46</b>	The interface treatment and service lane access of Ison Road and the Government Primary School and LTC must be to the satisfaction of the responsible authority and VicRoads.
<b>R47</b>	Building facades on the State Government P-6 School site and adjacent Community Centre site must address and activate the streets as indicated on Plan 8 and be sited on or within 5.0 meters of the front property boundary.
<b>R48</b>	Buildings must not adversely overshadow vegetated riparian banks.
<b>R49</b>	Building setbacks from waterways and wetlands must be informed by Melbourne Water's Waterway Corridor Guidelines.
<b>R50</b>	Subdivision and development in town centre must also address the design principles and performance criteria outlined in Appendix 4.3.

GUIDELINES

LOCAL TOWN CENTRE	
<b>G34</b>	<p>The LTC should be located generally in accordance with the locations and use terms identified on the Local Town Centre concept plan 8 and as follows:</p> <ul style="list-style-type: none"> <li>• A maximum of one full line supermarket which may be supported by a smaller supermarket or small anchor retail premises;</li> <li>• Potential for additional major and minor retail anchors subject to future planning permits;</li> <li>• Supporting specialty stores;</li> <li>• Cafe, restaurant and take-away premises;</li> <li>• Commercial locations located along Ison Road and the LTC connector street which may include offices, medical, childcare and SOHO (Small Office Home Office) uses;</li> <li>• The provision for other commercial uses along Ison Road such as business/warehouse units;</li> <li>• Mixed use precincts may include retail, home/office, cafe/restaurant or residential uses, as well as school and community uses;</li> <li>• Car parking;</li> <li>• Boat ramps and jetties;</li> <li>• Medium and high density housing;</li> <li>• A major public space node which acts as a regional destination and offers a range of passive and active open space activities and may also contain cafes, restaurants and community facilities;</li> <li>• A Government Primary School and Council delivered Community Facility;</li> <li>• A central 'urban' public meeting space such as a town square or similar is directly linked to retail core / waterfront;</li> <li>• Open space along the promenade is to provide flexible all purpose usage for a broad range of activities including markets, public art, community, recreation and leisure uses.</li> </ul> <p>Key locations within the Local Town Centre should include features of interest incorporated into the built form and landscape outcomes (refer to the Local Town Centre Concept Plan 8).</p> <p>Significant built form may include:</p> <ul style="list-style-type: none"> <li>• Minimum two storey construction or elements of two storey construction (such as higher floor to ceiling heights, parapets, awnings, shade structures or other roof elements);</li> <li>• Sculptured facades which include recesses and projections to provide variation and segmentation to the building facade;</li> <li>• Strong vertical elements;</li> <li>• Balconies;</li> <li>• Roof and/or wall articulation; and/or</li> <li>• Feature colours or materials which are sympathetic to their surrounds.</li> </ul>
<b>G35</b>	<ul style="list-style-type: none"> <li>• Minimum two storey construction or elements of two storey construction (such as higher floor to ceiling heights, parapets, awnings, shade structures or other roof elements);</li> <li>• Sculptured facades which include recesses and projections to provide variation and segmentation to the building facade;</li> <li>• Strong vertical elements;</li> <li>• Balconies;</li> <li>• Roof and/or wall articulation; and/or</li> <li>• Feature colours or materials which are sympathetic to their surrounds.</li> </ul>
<b>G36</b>	Consideration should be given to pedestrian movement north-south across Ison Road and east-west across the local town centre connector street. Opportunities for a dedicated pedestrian crossing in both of these locations should be explored in conjunction with determining bus stop locations.
<b>G37</b>	A consistent and unified landscape treatments should be provided on the interface between the LTC and Ison Road.
<b>G38</b>	The installation of and use of rainwater tanks to capture water for the toilet flushing and local irrigations should be provided in the LTC.

ESTIMATED PRECINCT EMPLOYMENT SUPPLY				
Land Use	Measure	Jobs	Qty in PSP	Est. Jobs
Multi purpose community centre, including 2 Kindergartens	Jobs/centre	20	1	20
Private Child Care centre	Jobs/100 places	15	1	15
Primary School	Job/school	40	2	80
Secondary School	Job/school	90	1	90
Retail	Job/sq m	1 per 30 sq m	8,000	267
Office/non retail commercial	Job/sq m	1 per 20 sq m	2,400	120
Home Based Business	Job/dwelling	0.05	5,200	260
<b>Total</b>				<b>852</b>

# BLACK FOREST ROAD SOUTH PRECINCT STRUCTURE PLAN

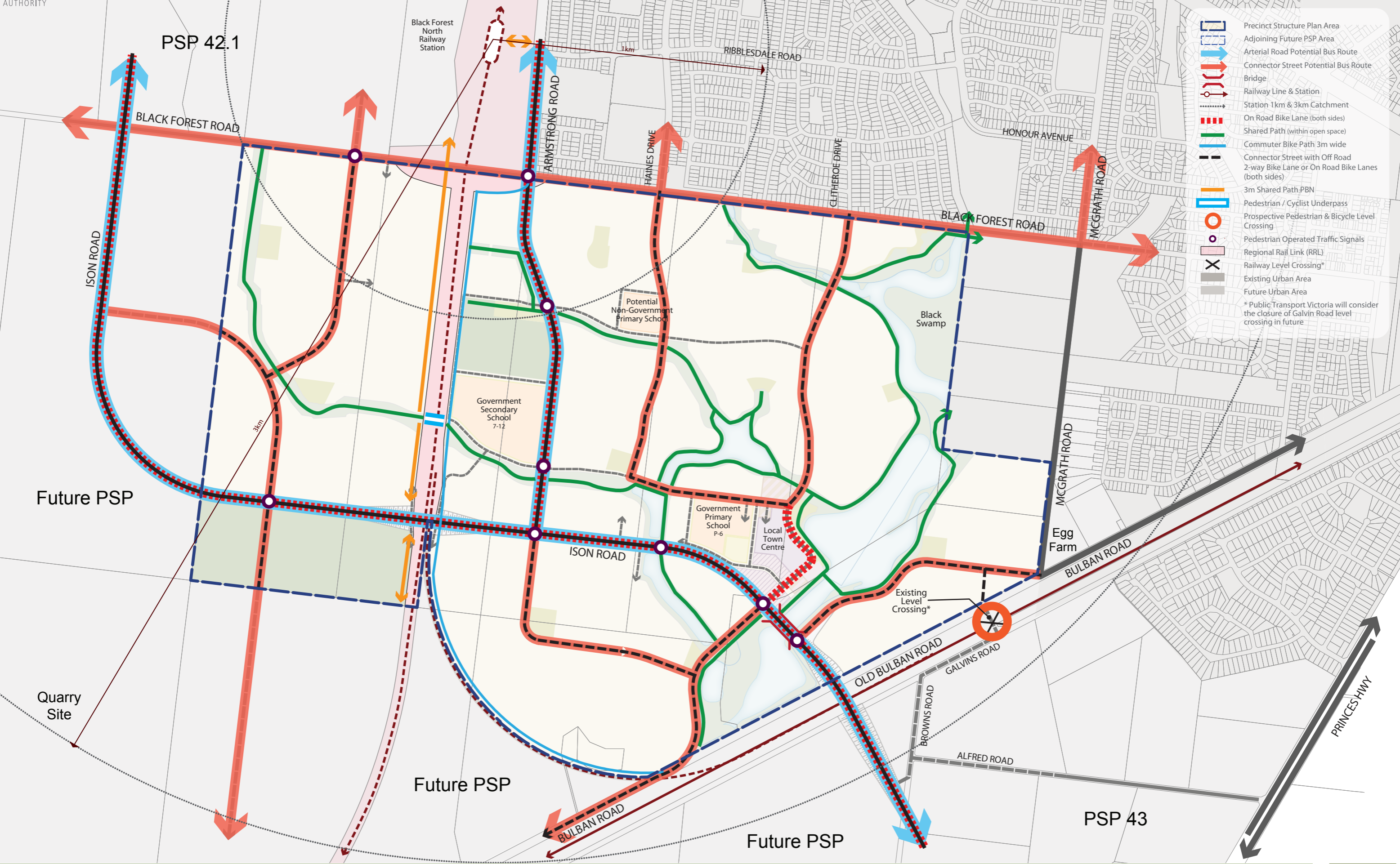


- Precinct Area
  - Primary Arterial Road - 6 Lane (41m) - See CS1
  - Secondary Arterial Road - 4 Lane (34m) - See CS1/2
  - Connector Street Black Forest Road (25m) - See CS4
  - Connector Street Black Forest Road (25m) - See CS5
  - Boulevard Connector Street (32m) - See CS6
  - Connector Street (25.5m) - See CS7
  - Connector Street (23.6m) - See CS8
  - Local Town Centre (LTC) Connector (22m) - See CS9
  - Local Town Centre (LTC) Connector (23.3m) - See CS10
  - Access Street Level 2 (20m) - See CS11 - Interface to Ison Road & P6 School
  - Access Street Level 2 (21.1m) - See CS12 - Interface to Secondary School
  - Access Street Level 2 (20m) - See CS13
  - Access Street Level 1 (17m) - See CS14 - Interface to Regional Rail Link & Metro Spur
  - Access Street Level 1 (16m) - See CS15
  - Bridge
  - Railway Line
  - Proposed Railway Line & Station
  - Regional Rail Link (RRL)
  - Railway Level Crossing\*
  - Signalised Intersection
  - Pedestrian Signalised Crossing - Development Works
  - Roundabout
  - Key Roundabout Small - Development Works
  - T Intersection - Development Works
  - Left In/Left Out Intersection
  - Future Grade Separation (subject to VicRoads referral)
  - Planned Grade Separation (by RRLA)
  - Existing Urban Area
  - Future Urban Area
- \* Public Transport Victoria will consider the closure of Galvin Road level crossing in future

**Plan 9**  
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Plan 10

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Public Transport & Walking Trails

## 3.4 TRANSPORT, WATER AND UTILITIES

## REQUIREMENTS

## TRANSPORT: PUBLIC TRANSPORT

- R51** Bus stop facilities must be constructed by development proponents as part of the subdivision works (prior to the issue of a statement of compliance for the relevant stage) in accordance with the requirements of the Public Transport Guidelines for Land Use and Development to the satisfaction of the Director of Public Transport.
- R52** The bus stop facilities must be provided with DDA compliant direct and safe pedestrian access connected to an existing pedestrian / shared path.
- R53** The bus stop facilities must be designed as an integral part of town centres and activity generating land uses, such as schools, sports fields and employment areas.

## GUIDELINES

## TRANSPORT: PUBLIC TRANSPORT

- G39** Where required, appropriate fencing to protect the Geelong – Melbourne and RRL rail corridors should be provided having consideration with the V/Line fencing standards SAST-1 Fencing Right of Way – Project New Construction.

## REQUIREMENTS

## TRANSPORT: STREET NETWORK

- R54** Street layouts of individual subdivisions must integrate to:
- Form a coherent movement network across the wider precinct; and,
  - Ensure no dwelling is disadvantaged by poor access to open space or facilities.
- R55** The arterial road network and the land required to facilitate regional connections through the precinct and more broadly Wyndham West, must be provided as early in the development of the land as is practicable.
- R56** Staging of subdivisions must provide for the timely connection of the Arterial Road network and specifically Ison Road and Armstrong Road and seek to co-ordinate the delivery of these roads in conjunction with the timing of arterial road connections located external to this precinct.
- R57** Staging of subdivisions must provide for the timely connection of roads and off-road pedestrian and cycle networks between properties.
- R58** In each stage, roads must be constructed to property boundaries where inter-parcel connections are indicated in the structure plan.
- R59** Driveway access to lots fronting arterial or sub-arterial roads must be provided from local roads or rear lanes only. Service roads may be considered in exceptional circumstances, subject to agreement of the coordinating road authority.
- R60** Access arrangements to Ison Road must be to the satisfaction of the responsible authority and VicRoads.
- R61** Provide a local road on the east side of RRL that in the interim, intersects with Ison Road. Ultimately this road and Ison Road are to be grade separated once Ison Rd crosses the RRL.
- R62** Provide a local road on the west side of RRL that in the interim intersects Ison Road and connects into the active open space facilities. Ultimately this road and Ison Rd are to be grade separated once Ison Road crosses the RRL.
- R63** Roads must be constructed generally in accordance with the relevant road cross section in this Precinct Structure Plan unless otherwise agreed by the responsible authority.

## GUIDELINES

## TRANSPORT: STREET NETWORK

- G40** Street block lengths should not exceed 240 metres to ensure a permeable and low speed environment for pedestrians, cyclists and vehicles is achieved.
- G41** Street layouts should provide multiple convenient routes to the LTC to disperse traffic across the precinct and reduce the potential for congestion.
- G42** Use of cul-de-sacs should not detract from convenient pedestrian and vehicular connections.
- G43** Intersections of local connector streets and arterial roads should be designed to facilitate the safe and convenient movement of all transport modes.
- G44** The alignment and layout of the road network should accord with Plan 9, however consider variation and flexibility of this layout so long as the intended performance and function of the network is maintained to the satisfaction of Council and VicRoads.
- G45** The local street network should facilitate convenient access to the LTC by ensuring a permeable and efficient network of connecting streets.

## REQUIREMENTS

## TRANSPORT – WALKING &amp; CYCLING

- R64** Walking and cycling networks, including bridges across waterway corridors must be constructed by development proponents as part of subdivision works relevant to that stage boundary, prior to the issue of a statement of compliance for the relevant stage.
- R65** Pedestrian crossings of waterways must be clear-span bridges or boardwalks to the satisfaction of Melbourne Water.
- R66** All pedestrian crossings and pathways in waterway corridors to be managed by Melbourne Water must be designed to the satisfaction of Melbourne Water.
- R67** All pedestrian crossings of the Lollypop Creek corridor must be above the 1:100 year flood level.
- R68** Pedestrian paths must be provided on both sides of connector and access streets in accordance with the cross-sections in this PSP, unless adjacent to open space areas, where pedestrian paths may be incorporated into the open space design.
- R69** Pedestrian and cycle crossing points must be provided at all intersections and on key desire lines.
- R70** Bicycle parking facilities must be provided by development proponents in convenient locations at key destinations such as parks and town centres, to the satisfaction of the responsible authority.
- R71** A shared path must be provided on the east side of the RRL providing a continuous link to the future train station to the north of Black Forest Road.

## GUIDELINES

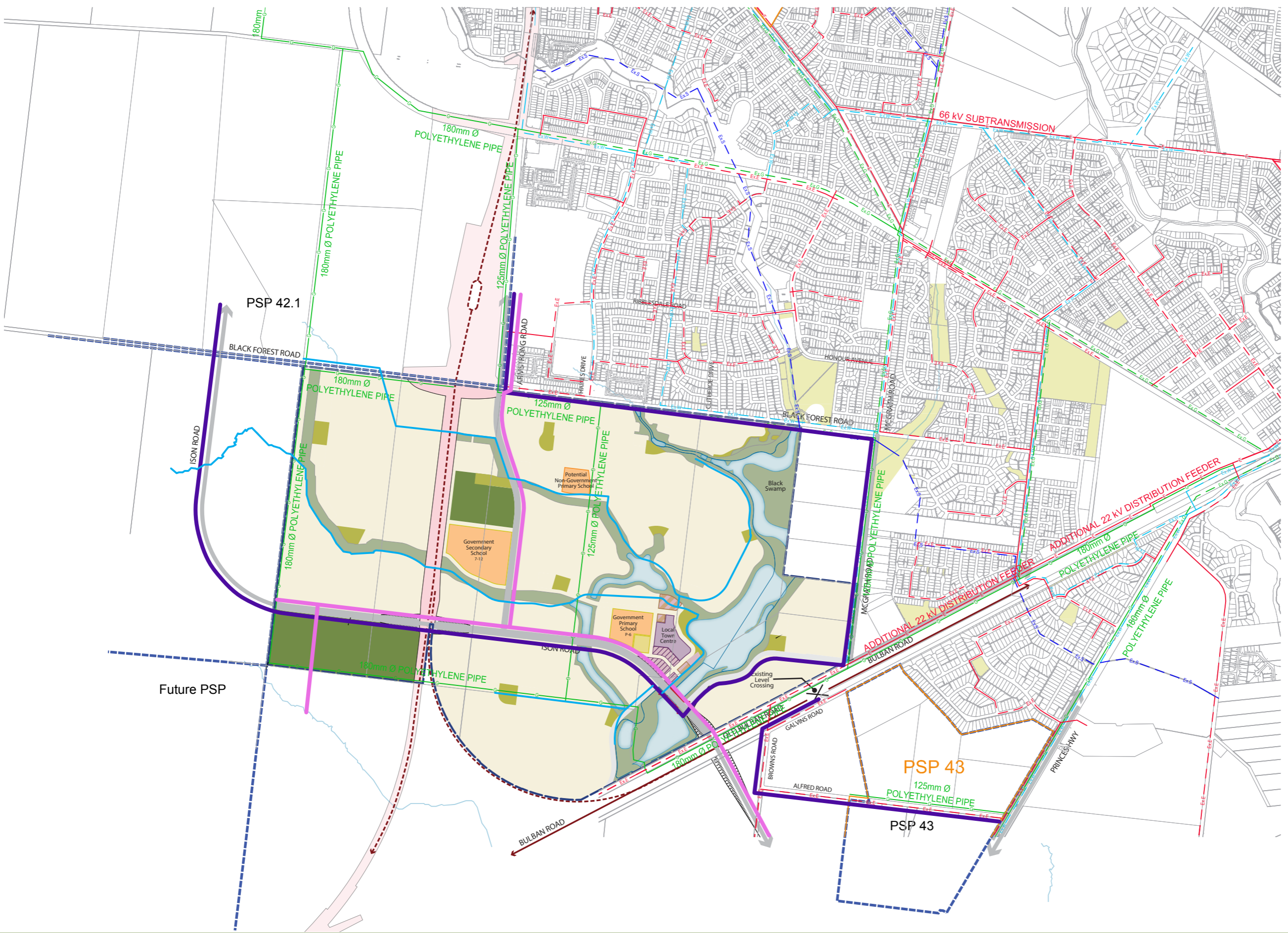
## TRANSPORT: WALKING &amp; CYCLING

- G46** Cycle connections should be designed to allow for the safe and convenient transition between on-road and off-road networks.

## REQUIREMENTS

## WATER/DRAINAGE &amp; INTEGRATED WATER MANAGEMENT

- R72** Final design of all major drainage infrastructure waterway corridors, retarding basins and wetlands must be to the satisfaction of the responsible authority.
- R73** Third pipe recycled water infrastructure must be provided to each lot.
- R74** Any drainage works must not unreasonably reduce existing water flow rates, particularly 'low flow' rates, from the Lollypop Creek/Werribee River breakaway confluence to land south of the Geelong-Melbourne rail line.
- R75** Stormwater management infrastructure must be designed in accordance with the Black Forest Road Development Services Scheme established by Melbourne Water.
- R76** Stormwater quality treatment must be to the satisfaction of Wyndham City and Melbourne Water. The strategy must consider Best Practice Environmental Management targets for discharge into waterways to best practice standards or to the satisfaction of Melbourne Water.
- R77** Stormwater flow regimes should be maintained at levels as close as possible to pre-development levels, to the satisfaction of Melbourne Water. Where specific biodiversity requirements for flow regimes apply, flow regimes must meet the requirements of responsible authorities.
- R78** Development must conform to relevant policies and strategies being implemented by the responsible authority, Melbourne Water and the Water Retail Authority, including any approved Integrated Water Management Plan.
- R79** Subdivision applications must demonstrate how integrated water management initiatives have been incorporated to reduce reliance on potable water and increase the utilisation of storm and waste water that contributes to a sustainable and green urban environment.
- R80** Subdivision applications must demonstrate how drainage and integrated water management initiatives maximise the multiple use of land for recreation and / or environmental purposes.
- R81** Any stormwater management plan prepared must demonstrate how the stormwater treatment system has been designed to ensure Growling Grass Frog wetlands, and other areas of generally suitable habitat, will receive water of adequate quality and quantity to enable the local persistence of the species. In some instances this may require treatment to levels above current best practice for the treatment of urban run off.
- R82** Waterways and drainage infrastructure must be designed with a suitable buffer from urban development and provide for ephemeral water bodies to enable the replication of natural flows and provide habitat for local species.
- R83** Corridors and buffers along waterways must be designed to protect water quality and public health and safety to Melbourne Water's satisfaction and as informed by Melbourne Waterways Corridor Guidelines.



## REQUIREMENTS

## WATER/DRAINAGE &amp; INTEGRATED WATER MANAGEMENT

- R84** Within the Town Centre and medium or high density residential areas abutting a waterway, gross pollutant traps must be fitted on all drain entry points.
- R85** Integrated water management systems should be designed to maximise habitat values for local flora and fauna species.

## GUIDELINES

## WATER/DRAINAGE &amp; INTEGRATED WATER MANAGEMENT

- G47** Integrated water management systems should be designed to maximise habitat values for local flora and fauna species and enable future harvesting and / or treatment and re-use of storm water where practical.
- G48** Developers shall consider installation of integrated localised stormwater harvesting systems to the satisfaction of responsible authority.

## REQUIREMENTS

## UTILITIES

- R86** All new electricity supply infrastructure (excluding substation and cables with a voltage greater than 66kv) must be provided underground.
- R87** New substations must be identified at the subdivision design response stage to ensure effective integration with the surrounding land uses and to minimise amenity impacts.
- R88** Utility services must not be located within conservation areas including Lollipop Creek reserves, its tributaries and Black Swamp.

## GUIDELINES

## UTILITIES

- G49** Utility service substation/kiosk sites must not be located on any land identified as public open space or to be used for any municipal purpose unless otherwise agreed in writing or as part of a planning permit application by the responsible authority.
- G50** The design of subdivision electricity infrastructure must consider the practicality of removing existing above ground electricity lines in the local and arterial road network both within and abutting the subdivision and re-routing lines underground through the subdivision.

## REQUIREMENTS

## DEVELOPMENT STAGING

- R89** The arterial road network and the land required to facilitate regional connections through the precinct and more broadly Wyndham West, must be provided as early in the development of the land as is practicable.
- R90** Staging of subdivisions must provide for the timely connection of the Arterial Road network and specifically Ison Road and Armstrong Road and seek to co-ordinate the delivery of these roads in conjunction with the timing of arterial road connection located external to this PSP.

## GUIDELINES

## DEVELOPMENT STAGING

- Staging will be determined largely by the development program of proponents within the Precinct and the availability of infrastructure services. Within this context, the following should be achieved:
- Development staging should not create circumstances in which residents will be unreasonably isolated from community facilities or public transport.
  - Development staging should, to the extent practicable, be integrated with adjoining developments, including the timely provision of connecting roads and walking/ cycling paths.
  - Access to each new lot is to be via a sealed road.
- It is likely the initial developments will occur from the north-east section and north-west section of the Precinct.
- G51**
- G52** The early delivery of active open space, community facilities, local parks and playgrounds should be encouraged within each neighbourhood and may be delivered in stages.

## 3.5 SUBDIVISION CONSTRUCTION WORKS

## REQUIREMENTS

## INFRASTRUCTURE DELIVERY

As part of subdivision construction works, new development must provide and meet the total cost of delivering the following infrastructure:

- Connector roads and local streets.
  - Local bus stop infrastructure.
  - Landscaping of all existing and future roads and local streets.
- R91**
- Intersection works and traffic management measures along arterial roads, connector roads, and local streets (except those included in the Development Contributions Plan).
  - Council approved fencing and landscaping (where required) along arterial roads.
  - Local pedestrian and bicycle paths along local arterial roads, connector roads and local streets and within local parks (except those included in the Development Contributions Plan).
  - Bicycle parking facilities in convenient locations at key destinations such as parks and activity centres.
  - Local drainage systems and associated pedestrian bridges.
  - Infrastructure as required by utility services providers including water, sewerage, drainage (except where the item is funded through a Development Services Scheme), electricity, gas, and telecommunications.

## Open Space Delivery

All parks (where not otherwise provided via a Development Contributions Plan) must be finished to the following standards to the satisfaction of the responsible authority prior to the transfer of the space to Council:

- Removal of all existing disused structures, foundations, pipelines or stockpiles.
  - Cleared of rubbish and environmental weeds, surface and protruding rocks, levelled, topsoiled and grassed with warm climate grass (unless conservation reserve requirements dictate otherwise).
  - Provision of water tapping potable and recycled water connection points suitable for reserves. Sewer and gas connection points must also be provided to land identified as an active reserve.
- R92**
- Drought tolerant plantings.
  - Vehicles exclusion devices (fence, bollards or other suitable method) and maintenance access points unless an alternative solution is agreed with the responsible authority.
  - Construction of a 2.5 metre shared path (concrete unless otherwise approved by the responsible authority) around the perimeter of the reserve, connecting and linking into any other surrounding paths or points of interest.
  - Installation of park furniture including BBQs, shelters, tables, local scale playgrounds and other local scale play elements such as ½ basketball courts and hit-up walls, rubbish bins and appropriate paving to support these facilities.
  - For town squares and urban spaces – paving and planters, furniture including seating, shelters and bollards, tree and other planting, lighting, drainage and water tapping.

- R93** Subject to City West Water agreeing to do so, the developer must enter into an agreement with City West Water requiring the subdivision to be reticulated with a dual pipe recycled water system to provide for the supply of recycled water from a suitable source to all lots and open space reserves provided in the subdivision.

Active open space required to be set aside by a Development Contributions Plan must be vested in the relevant authority in the following condition:

- R94**
- Free from surface / protruding rocks and structures;
  - Reasonably graded and / or topsoiled to create a safe and regular surface (with a maximum 1:6 gradient); and Bare, patchy and newly graded areas seeded, top-dressed with drought resistant grass.

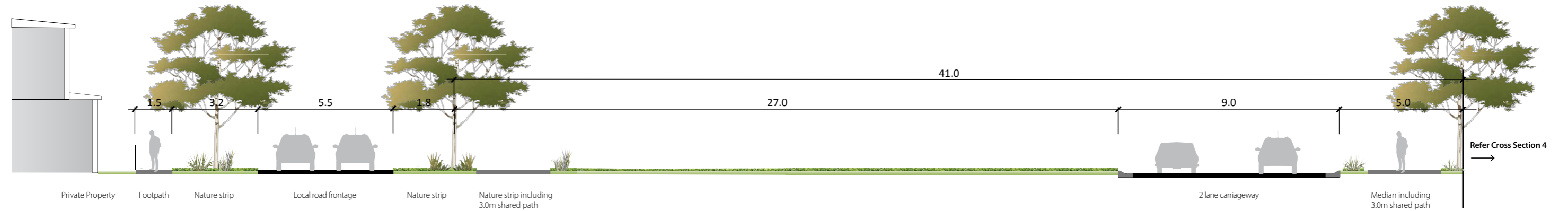
## REQUIREMENTS

## LAND BUDGET

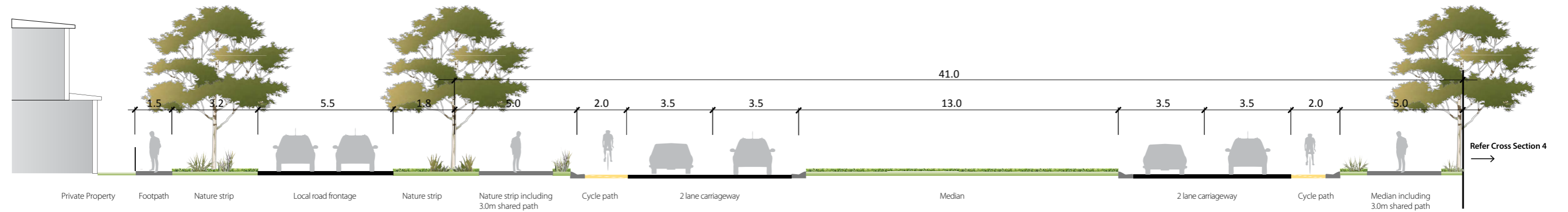
- R95** The detailed land budget included in Section 2.3 Table 3: Property Specific Land Use Budget clearly sets out the NDA for every property included in the PSP. The NDA will not be amended to respond to minor changes to land budgets that may result from the subdivision process unless the responsible authority agrees to a variation.

**Cross-Section 1: Primary Arterial Road 6 Lane (41m) (40000 vpd)**  
Ison Road

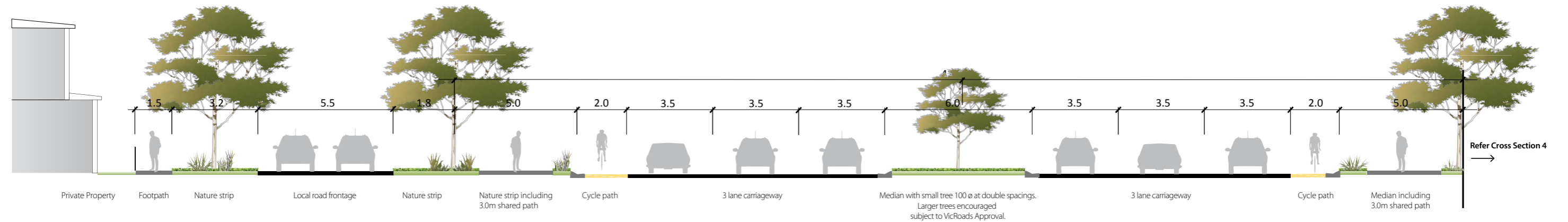
**INTERIM DESIGN SOLUTION**



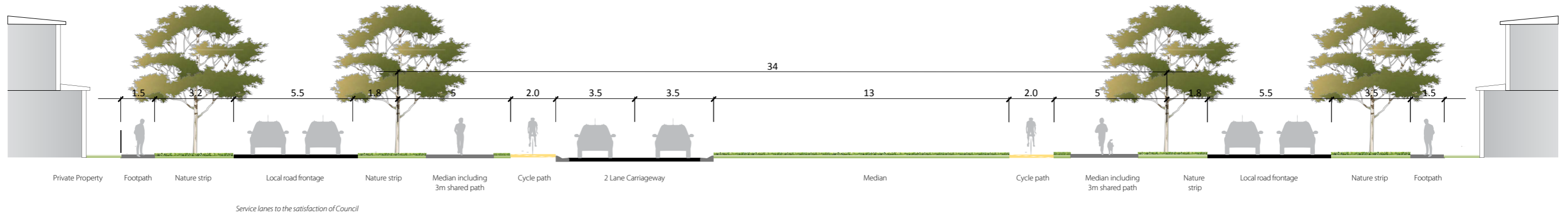
**DUPLICATION DESIGN SOLUTION**



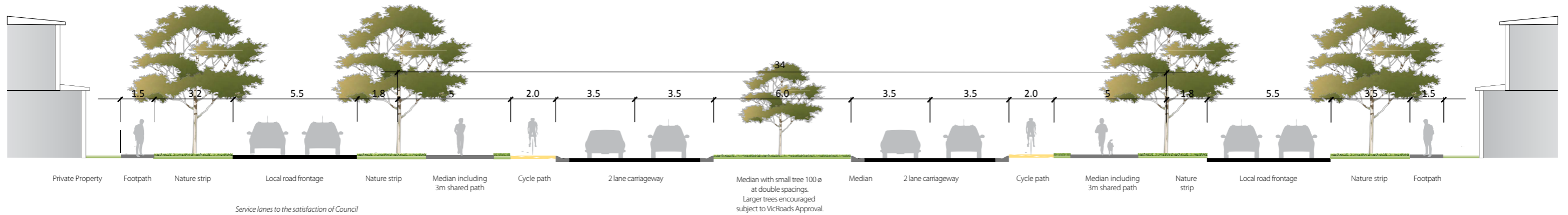
**ULTIMATE DESIGN SOLUTION**



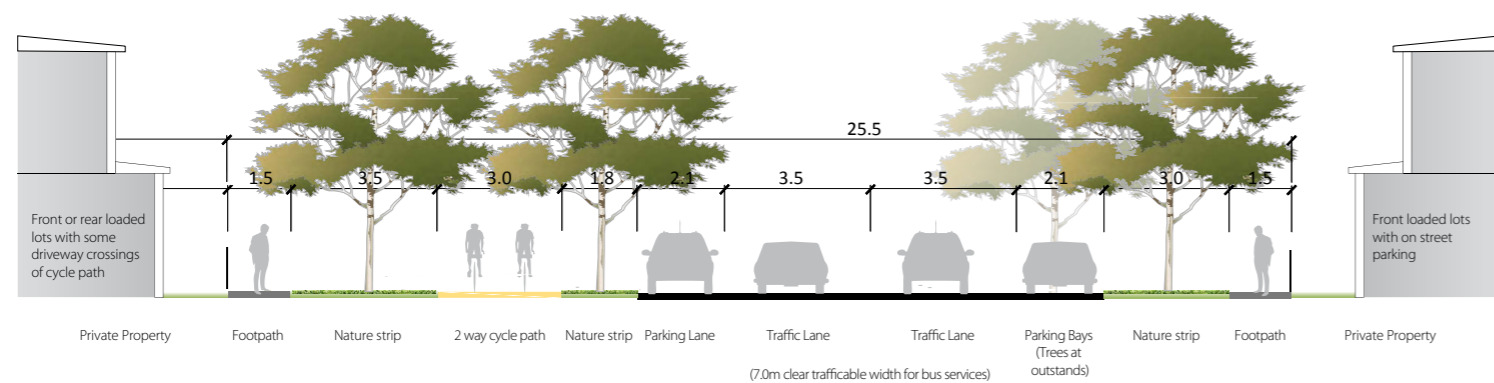
**Cross-Section 2: Secondary Arterial Road 4 Lane (34m)** (12,000-40,000 vpd)  
Interim Design Solution



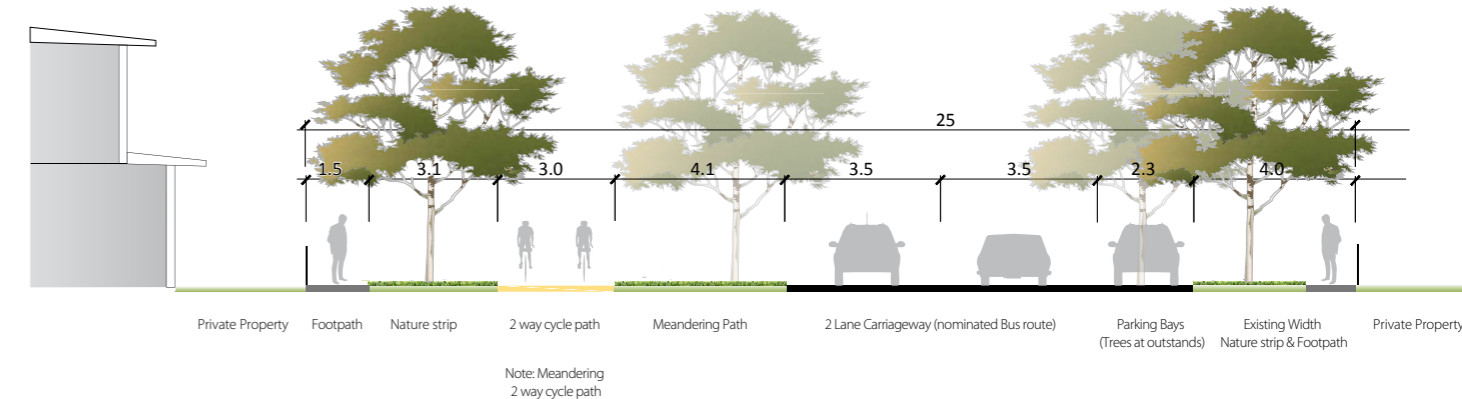
**Cross-Section 3: Secondary Arterial Road 4 Lane (34m)** (12,000 - 40,000 vpd)  
Ultimate Design Solution



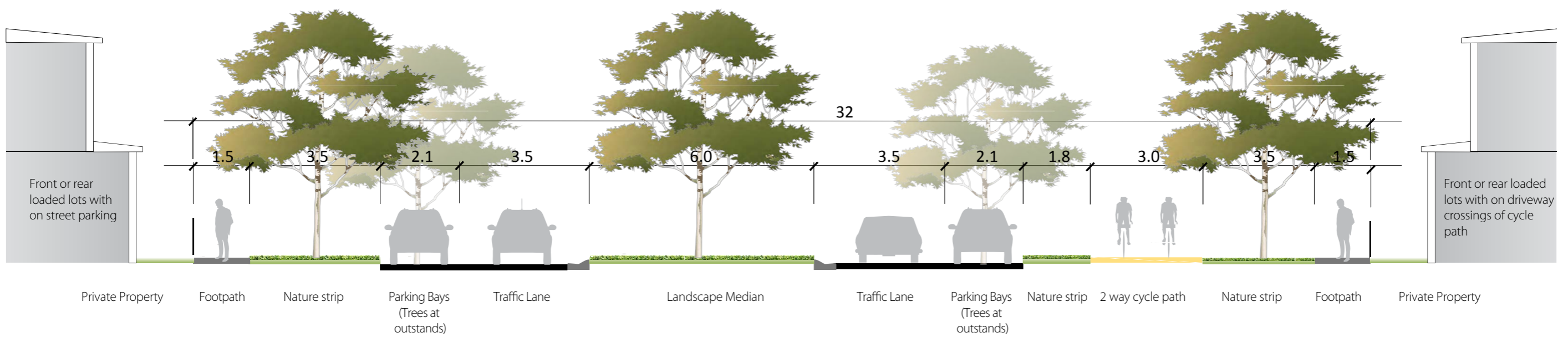
**Cross-Section 4: Connector Street (25.5m)** (<7,000 vpd)  
Black Forest Road (From RRL to the west end of PSP)



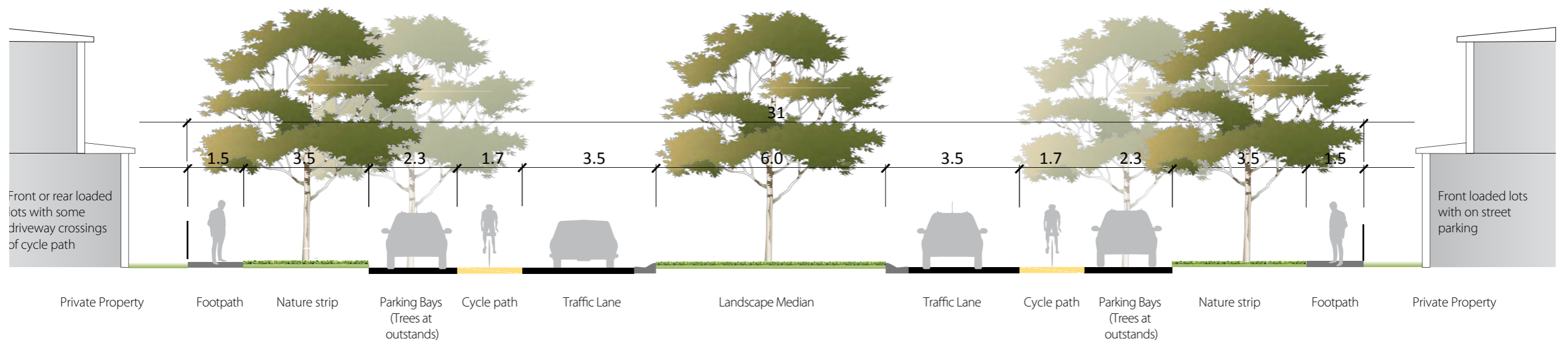
**Cross-Section 5: Connector Street (25m)** (<7,000 vpd)  
From McGrath Road to RRL Overpass



Cross-Section 6: (A) Boulevard Connector Street (32m) (<12,000 vpd)\*



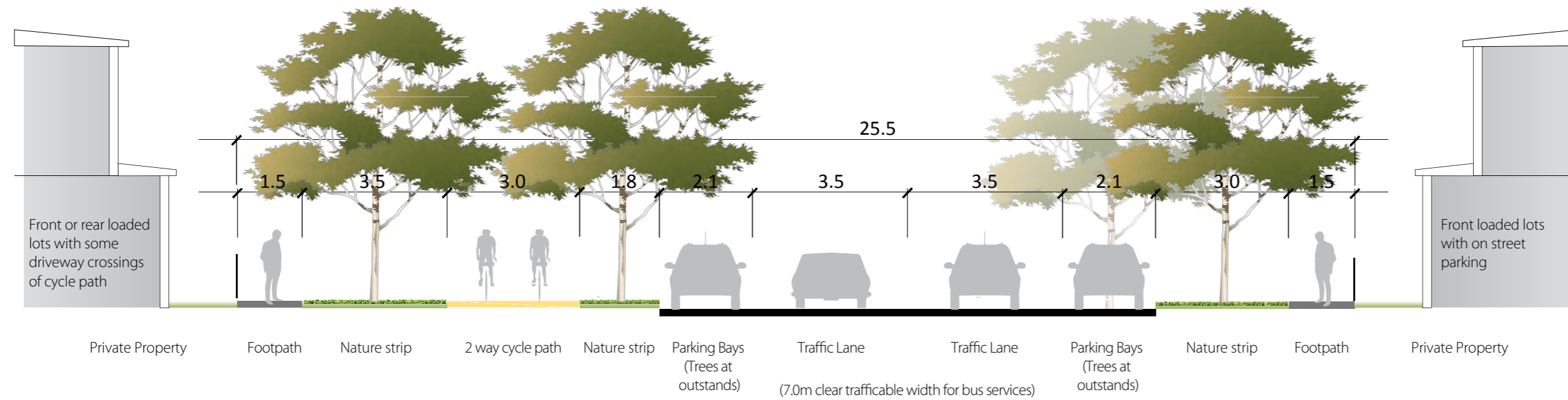
Cross Section 6: (B) Boulevard Connector Street with 6m median (31m) (<12,000 vpd)



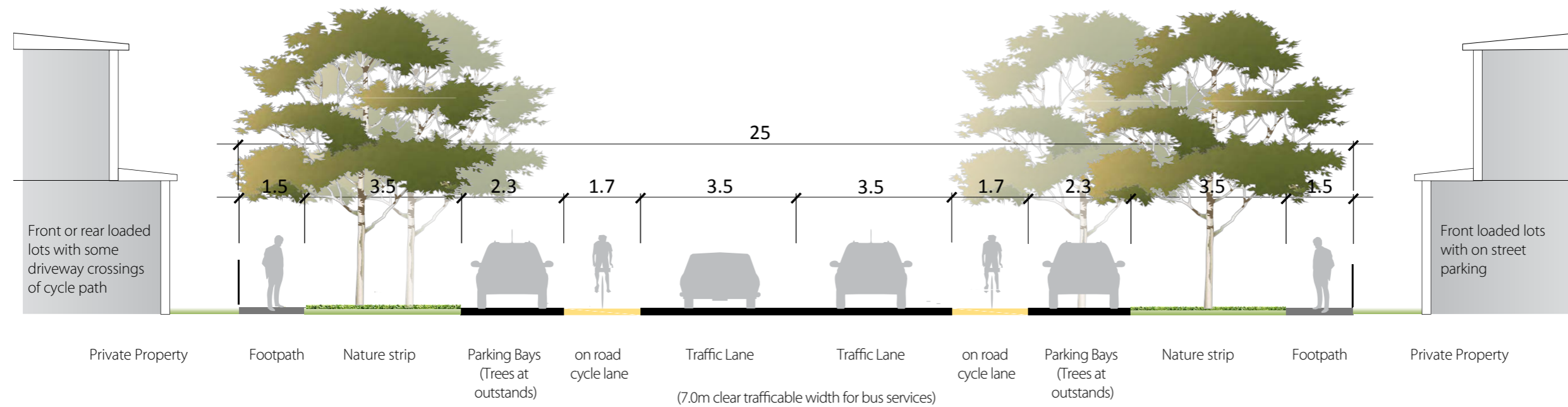
\* Note: Growth Areas Authority preferred cross section



**Cross-Section 7: (A) Connector Street (25.5m) (<7,000 vpd)**  
with Offroad Bicycle Shared Trail\*

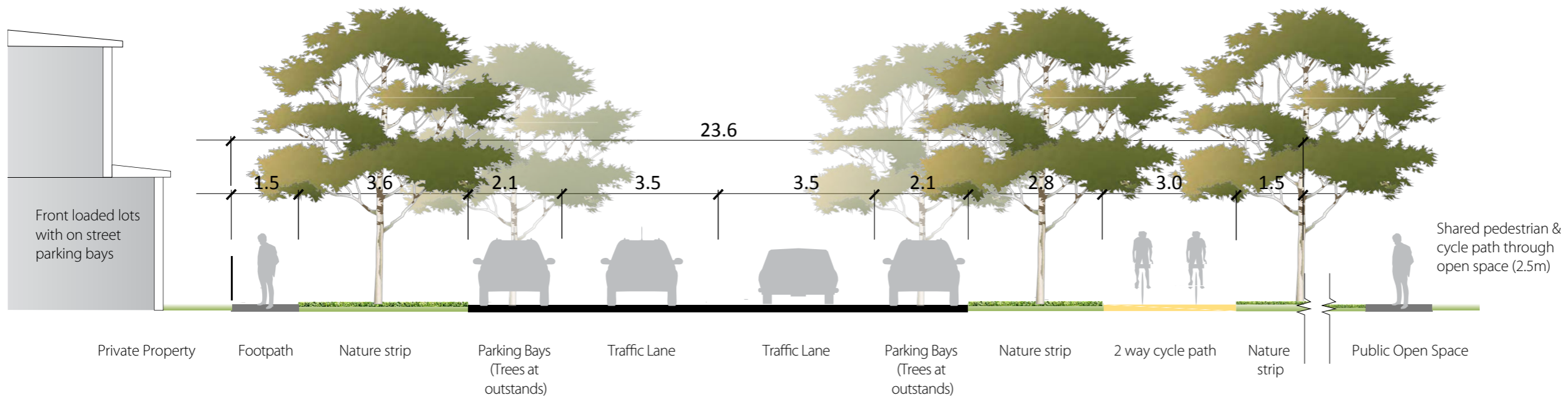


**Cross Section 7: (B) Connector Street - bikes on road (25m) (<7,000 vpd)**



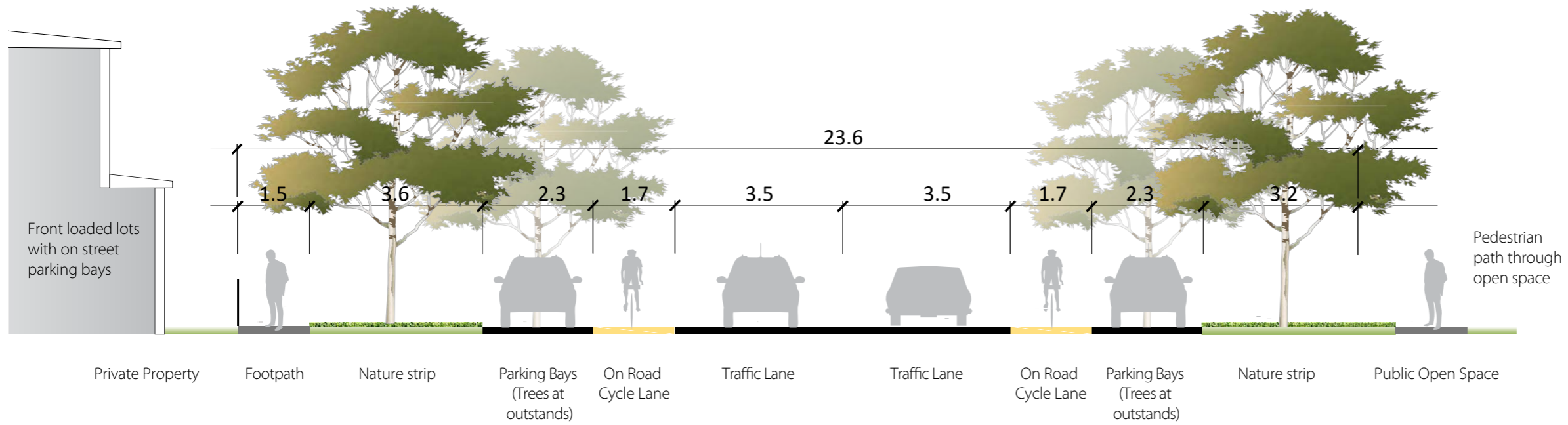
\* Note: Growth Areas Authority preferred cross section

**Cross-Section 8: (A) Connector Street, Bulban Road East (23.6m) (<7,000 vpd)**  
with open space interface & 2 way cycle path\*



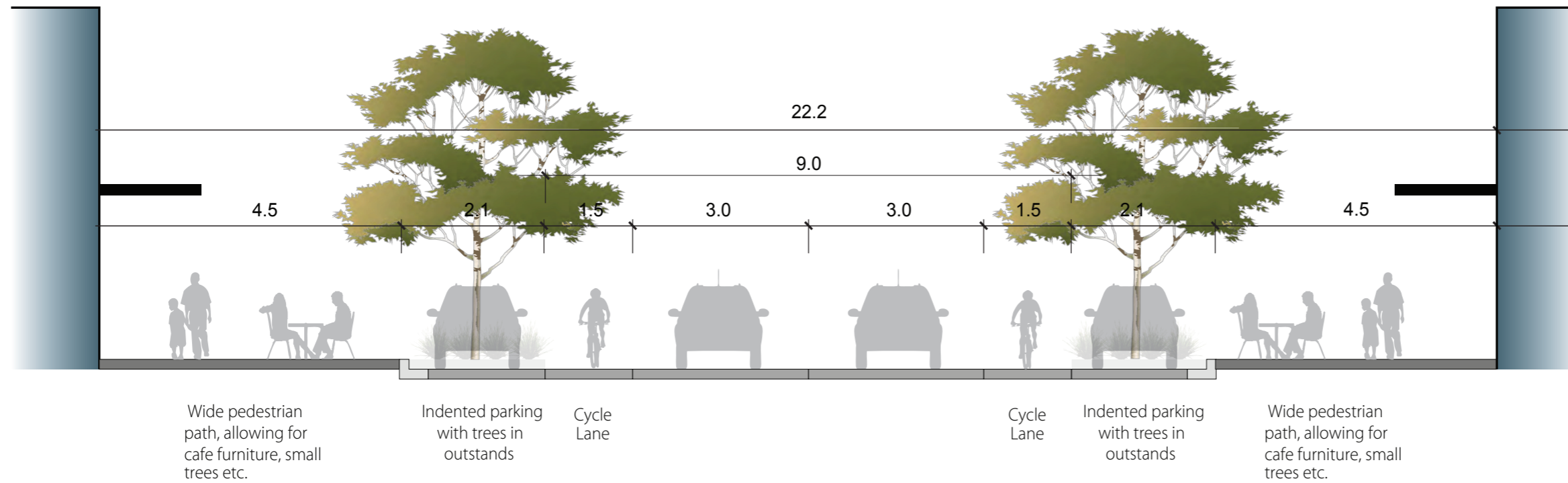
\*Remove parking lanes south of Connector Road

**Cross Section 8: (B) Connector Street, Bulban Road East - on-road cycle lanes (23.6m) (<7,000 vpd)**

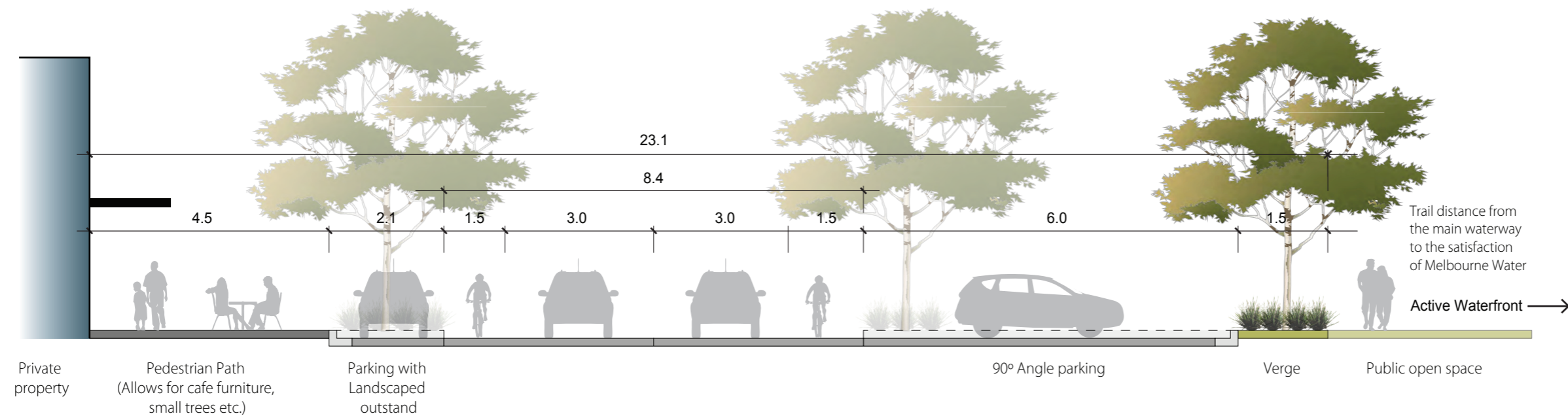


\* Note: Growth Areas Authority preferred cross section

**Cross-Section 9: Local Town Centre (LTC) Access Level 2 - Type 1 (22.2m) (2,000-3,000vpd)**



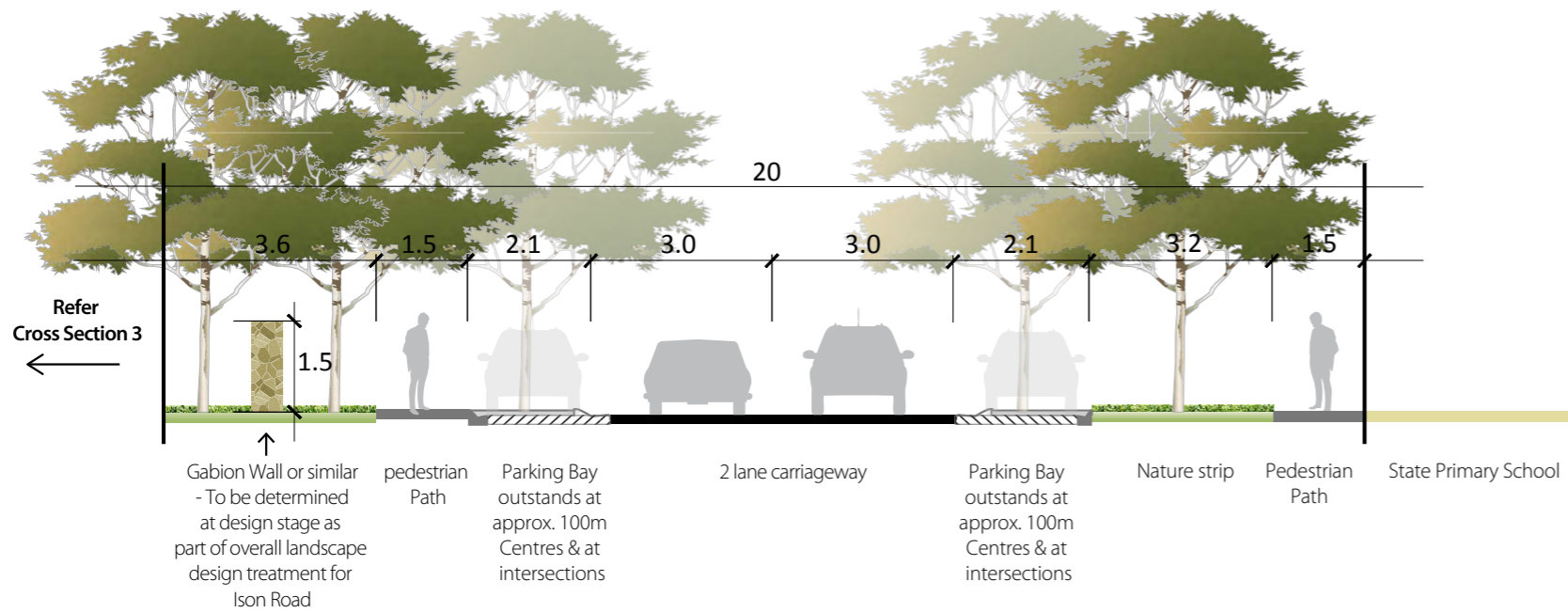
**Cross-Section 10: Local Town Centre (LTC) Access Level 2 - Type 2 (23.1m) (2,000-3,000vpd)**



**Notes:**

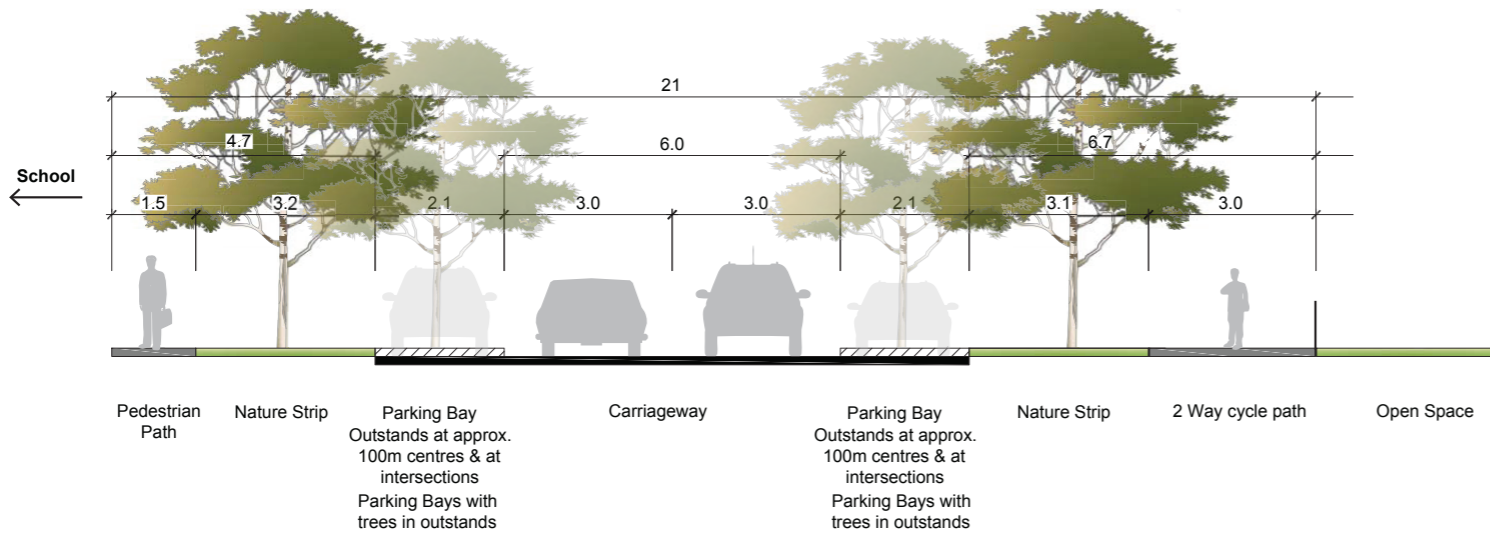
Cross-section design may vary within the road reserve width. Cross-section will be finalised at detailed design stage.

**Cross-Section 11: Access Street level 2 (20m) (<3,000 vpd)**  
Interface to Ison Road and P6 School



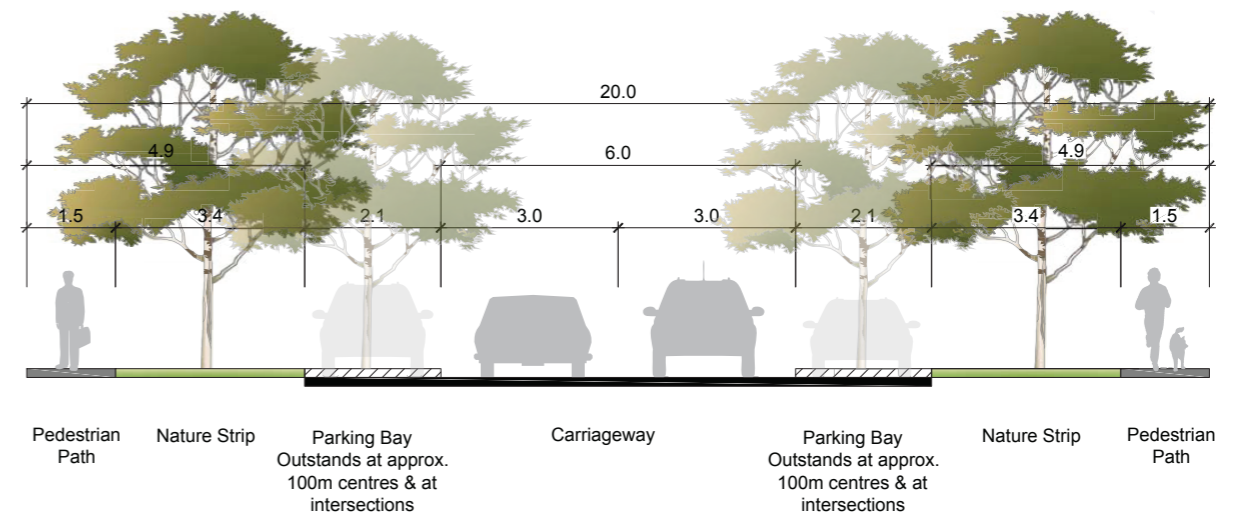
**Note: Drop off zones & pedestrian crossing must be pavement only with trees in grates**

**Cross-Section 12: Access Street Level 2 (21m) (2,000-3,000 vpd)**  
Interface to Secondary School

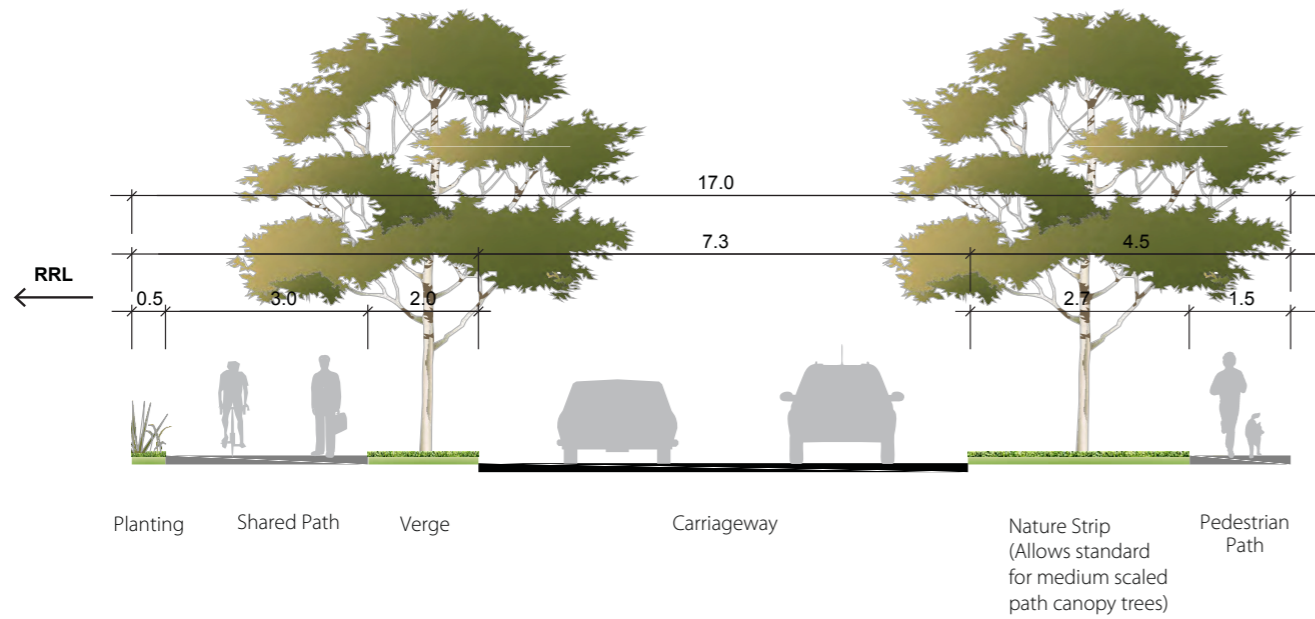


**Note: Drop off zones & pedestrian crossing must be pavement only with trees in grates**

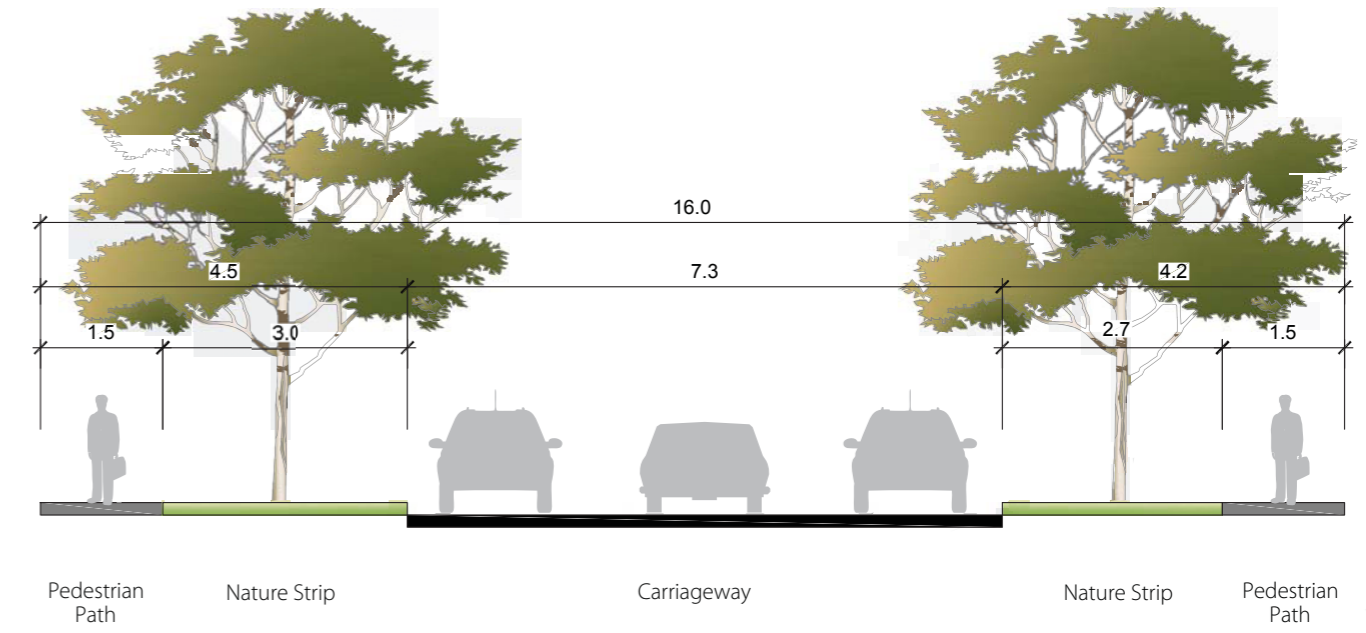
**Cross-Section 13: Access Street Level 2 (20m) (2,000-3,000 vpd)**



**Cross-Section 14: Access Street Level 1 (17m) (2,000 vpd)**  
Interface to Regional Rail Link and Metro Spur



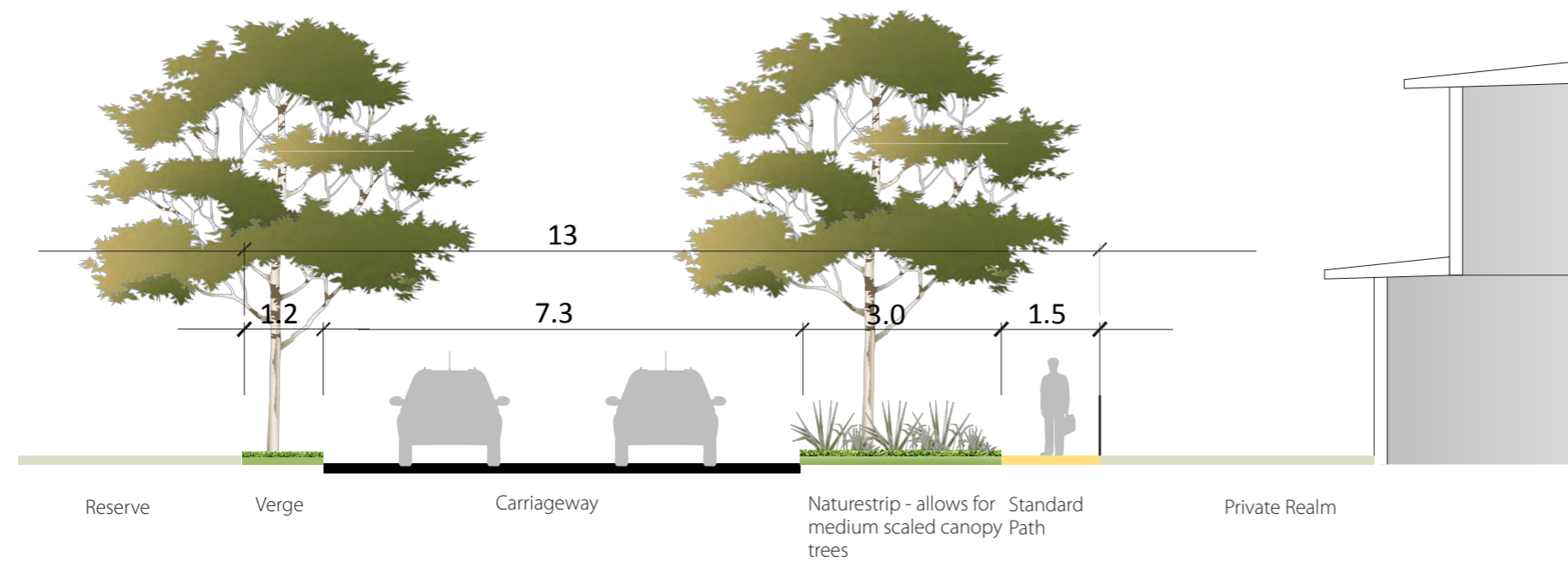
**Cross-Section 15: Access Place/Access Street Level 1 (16m) (< 2,000 vpd)**



**Notes:**

Shared path may be constructed on RRL and Metro Spur reserve if to the satisfaction of the Responsible Authority  
Services may have to be part located under shared path

**Cross-Section 16: Local Street with Reserve Frontage (13m) (2,000 vpd)**



### 3.6 PRECINCT INFRASTRUCTURE

The Precinct Infrastructure Plan (PIP) in Table 4 below sets out the infrastructure and services required to meet the needs of development of the precinct. The infrastructure items and services are to be provided through a number of mechanisms including:

- Subdivision construction works by developers;
- Agreement under Section 173 of the Act;
- Utility service provider requirements; and
- Capital works projects by Council, State Government agencies and non-government organisations.

**Table 5: Infrastructure required to support the development of the Precinct**

GROUP	CATEGORY	TITLE	DESCRIPTION	LEAD AGENCY	TIMING: S=2012-2017, M=2018-2030, L=2030+	INCLUDED IN WYNDHAM WEST DCP?
<b>ROAD CONSTRUCTION PROJECTS</b>						
Transport	Road	Ison Road.	Land for ultimate road configuration and construction of first carriageway	Wyndham City	S	Yes
Transport	Road	Construction for ultimate road configuration of Ison Road between Black Forest Road & existing railway line.	Construction of ultimate road configuration	VicRoads	S	No
Transport	Road	Ison Road crossing over Melbourne- Geelong Railway line	Land for bridge abutments either side of rail line	Wyndham City (land) VicRoads (construction of bridge) Melbourne Water (stormwater management)	S	Part
Transport	Road	Ison Road crossing over Melbourne- Geelong Railway line	Construction of bridge over rail reservation	Wyndham City (land) VicRoads (construction of bridge) Melbourne Water (stormwater management)	S	No
Transport	Bridge/culvert and bridge	Ison Road crossing over west drainage	Construction of bridge/culverts and bridge over west drainage area	VicRoads (construction of bridge)	S-M	Yes
Transport	Road	Ison Road crossing over RRL	Land for bridge abutments either side of rail line	Wyndham City (land)	S-M	Yes
Transport	Bridge	Ison Road crossing over RRL	Construction of bridge over rail reservation	VicRoads (construction of bridge)	S-M	No
Transport	Road	Armstrong Rd between Ison Road & Black Forest Road	Land for ultimate road configuration and construction of first carriageway	Wyndham City	S	Yes
Transport	Bridge/culvert	Armstrong Rd between Ison Road & Black Forest Road North	Bridge/culvert	Wyndham City	S	Yes
Transport	Bridge/culvert	Armstrong Rd between Ison Road & Black Forest Road South	Bridge/culvert	Wyndham City	S	Yes
Transport	Road	Black Forest Road	Black Forest Road: Additional land for ultimate road configuration including bridge over RRL	Wyndham City	S	Yes
Transport	Road	Black Forest Road	Construction of first carriageway	Wyndham City	S	Yes
Transport	Bridge	Black Forest Road	Construction of gade separation of Black Forest Road over RRL	RRLA	S	No
Transport	Road	New Bulban Road	Land for ultimate road configuration and construction of first carriageway	Wyndham City	S-M	Yes
Transport	Road	Extension to link to Galvin Road to new Bulban Road, land and construction of Galvin Road extension (north of Geelong Rail corridor)	Land for ultimate road configuration and construction of first carriageway	Wyndham City	S	Yes
Transport	Underpass	Pedestrian and bicycle underpass RRL	Constructions of the rail pedestrian and bicycle underpass RRL	Development Proponent/DCP	M	No

GROUP	CATEGORY	TITLE	DESCRIPTION	LEAD AGENCY	TIMING: S=2012-2017, M=2018-2030, L=2030+	INCLUDED IN WYNDHAM WEST DCP?
Transport	Pedestrian and bicycle crossing	Galvin/Brown road level crossing upgrade of pedestrian and bicycle crossing	Constructions of the Pedestrian and bicycle crossing	Wyndham City	S	Yes
<b>INTERSECTION PROJECTS</b>						
Transport	Road	Ison Road, Bulban Road & the local town centre connector staggered T intersection	Land for ultimate intersection configuration and construction of interim staggered T intersection	Wyndham City	S	Yes
Transport	Road	Ison Road & Armstrong Road and north/ south connector	Land for ultimate intersection configuration and construction of interim four way interim intersection	Wyndham City	S	Yes
Transport	Road	Ison Road & Armstrong Road	Land for ultimate intersection configuration and construction of interim four way interim intersection	Wyndham City	S	Yes
Transport	Road	Armstrong Road and East-West Connectors	Land and construction of ultimate four way intersection configuration	Wyndham City	S	Yes
Transport	Road	Armstrong Road and Black Forest Road	Construction of ultimate four way intersection configuration	Wyndham City	S	Yes
Transport	Road	Alfred & Geelong Roads Intersection	Signalisation of intersection	Wyndham City	S	Yes
Transport	Road	Black Forest Road and North-south connector	Land and construction of ultimate four way intersection configuration	Wyndham City	S	Yes
Transport	Road	Black Forest Road and Harpley Blvd	Land and construction of roundabout	Wyndham City	S	Yes
Transport	Road	Black Forest Road and Haines	Land and construction of roundabout	Wyndham City	S	Yes
Transport	Road	Bulban Road / McGrath Road	Land and construction of roundabout	Wyndham City	S	Yes
Transport	Road	Bulban Road / Galvin Road Extension	Land and construction of roundabout	Wyndham City	S	Yes
<b>RAIL</b>						
Transport	Rail	Rail infrastructure & services	Development of high capacity rail services initially as Regional Rail link	RRLA and PTV	S-L	No
Transport	Rail	Shared use path	Along the west side of the RRL	Development proponent	S-L	No
<b>BUS SERVICES</b>						
Transport	Bus services	Connector and arterial roads		PTV	M-L	No
<b>COMMUNITY FACILITIES</b>						
Community	Community	Located off north south local road in the Local Town Centre.	Land and construction of multipurpose community centre, including two kindergarten rooms and two maternal child health care rooms	Wyndham City	S	Yes
Community	Government Secondary School	Located next to the RRL and Armstrong Road	8.4 ha Government Secondary School	DEECD	S-M	No
Community	Government Primary P-6	Located next to the LTC	3.5 ha of land Government Primary P-6	DEECD	S-M	No
Community	Non Government Primary school	Located North of the PSP off the Haines Drive connector	2 ha of land for the Non Government Primary school	CEO	S-M	No
Community	Community	Town Centre Square	Construction of regional Open Space amenity including community amphitheatre	Wyndham City/ Development proponent	S-M	No

GROUP	CATEGORY	TITLE	DESCRIPTION	LEAD AGENCY	TIMING: S=2012-2017, M=2018-2030, L=2030+	INCLUDED IN WYNDHAM WEST DCP?
Community	Community	Town Centre Square	Provision of Land construction of Town Centre Square: Seating, lighting, hardscape, softscape, public art	Wyndham City/ Development proponent	S-M	No
<b>ACTIVE RECREATION RESERVES</b>						
Open space	Active	This active recreational area west of Armstrong Road will add value and supporting amenity co-located with the Secondary School.	Provision of Land: 8ha Active Open Space <ul style="list-style-type: none"> <li>x2 senior AFL / cricket ovals, cricket nets</li> <li>x1 district playground</li> <li>x160 car parking spaces</li> <li>x1 oval pavilion</li> </ul>	Wyndham City	M	Yes
Open space	Active	South west corner of the precinct bounded by the Regional Rail Link and South of Ison Road	Provision of Land: 22.8ha <ul style="list-style-type: none"> <li>x2 senior AFL / cricket ovals</li> <li>cricket nets</li> <li>x3 soccer pitches</li> <li>x12 tennis courts</li> <li>x8 softball Fields</li> <li>x4 netball courts</li> <li>district playground</li> </ul> <ul style="list-style-type: none"> <li>x240 car parking spaces</li> <li>x1 oval pavilion</li> <li>x1 soccer pavilion</li> <li>x1 tennis pavilion</li> <li>x1 netball pavilion</li> <li>x1 softball pavilion</li> </ul>	Wyndham City	M-L	Yes
Open space	Trails	Shared trail network	Construction of shared trail along Ison Road	Wyndham City	S	Yes
Open space	Trails	Shared trail network	Construction of shared trail along Armstrong Road	Wyndham City	S	Yes
Open space	Trails	Primary Public Transport Network (PPTN) routes	Construction of east shared trail along Regional Rail Link	Development Proponent	S	No
Open Space	Trails	Shared Link Network	Construction of a shared trail around regional wetland system including: <ul style="list-style-type: none"> <li>Pedestrian bridges</li> <li>Lighting Provision</li> </ul>	Development Proponent	S-M	Yes
Open Space	Active	Active Waterbody, east edge of Town Centre	Construction of boat launching facilities	Wyndham City/ Development Proponent	S-M	No



## 4.0 APPENDICES

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4.1 WATER DESIGN GUIDELINES

4.2 TOWN CENTRE

4.3 LOCAL TOWN CENTRE DESIGN PRINCIPLES

4.4 PLANTING GUIDELINES




## 4.1 WATER DESIGN GUIDELINES

The character of the waterbody and its interfaces are key to providing the 'livability by the water' setting for the Harpley community. The network of lakes and creeks will provide diverse wildlife habitats, wonderful views and provide valuable amenity spaces.

The waterbody is large in scale (~60Ha) and will encompass 4 distinct characters, each with a specific purpose. These purposes include:

- Natural and cultural conservation and education
- Formal setting for social and recreation activities
- Active water use
- Natural / Water retention / reuse

The following pages describe different edge treatments and how they can be designed to make safe, diverse, interesting and memorable places adjacent to the water.

-  Wetlands (Conservation and Constructed)
-  Formal active waterbody
-  Linear parks and waterways



Plan showing the different water bodies

### Active Waterbody

- Be designed to be offline from the waterway system
- Include appropriate litter and water quality treatment infrastructure to meet applicable EPA water quality standards
- Maintain flood storage and conveyance functions
- Any application for subdivision or works affecting a proposed active recreational water body must include a written agreement from an appropriate authority covering future ownership and management of the active recreational water body and related assets covering water quality, formal edging, and any recreational infrastructure. Design of water quality treatment, recreation facilities and any hard-edging for an active water body will need to the future owner and manager's satisfaction

### Formal Waterway

- WCC or Lend Lease own and manage any proposed walls, hard edges such as formal rocked, concrete, pier or board walk edging, to ensure these assets can be designed & any maintenance risks managed appropriately, and include requirements for litter management where hard edges are to be installed

### Wetlands (Conservation and Constructed)

The natural and constructed wetlands throughout Harpley are of significant cultural, ecological and hydraulic importance.

Design principles:

- Protect, and restore native vegetation and habitat.
- Provide sensitive visitor facilities to promote nature based recreation, such as walking trails, bird hides, informative signage and rest spots to the satisfaction of DSE

### Linear Parks and Waterways

The linear parks and waterways provide important local open space and off-road connectivity throughout Harpley. Linear parks and waterways provide important casual social interaction and recreation as well as habitat corridors that augment the ecological values of the wetlands.

Design principles:

- Provide a continue shared path along the linear park.
- Provide a range of spaces from manicured lawns to habitat planting.
- Promote streets as the edge condition too waterways and avoid any back or side fencing/ Walls abutting the waterway corridor. All Melbourne Water owned or managed waterway corridors must be fronted by an active edge including a road, trail or significant open space area

### Active Waterbodies



Rowing Clubs, Schools and Club



Boating Clubs - Sabot Sailing

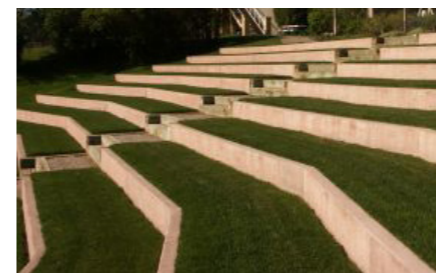


Boat Ramp

### Wetlands



Visual amenity focus



Social gathering spaces



Club House



Natural wetland edge

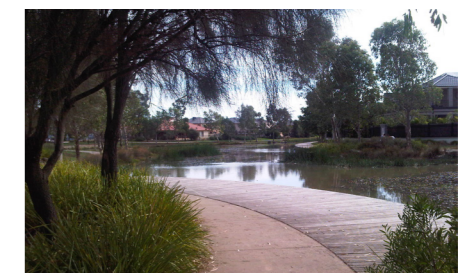


Look out area



Habitat appreciation areas

### Linear Parks & Waterways



Waterways



Habitat corridors



Resting opportunities

**Water Treatment**

Gross pollutant traps on all drainage points in town centres and all higher density residential areas will be necessary to maintain amenity and water quality in waterways – especially where hard edging is proposed. This may need to be addressed here as a design requirement

**Waters Edge Treatments**

**Appropriate safety benching must be designed**

All water edge design must adhere to Lend Lease policy, Melbourne Water, and the royal Life Saving Guidelines for Water Safety in Urban Water Developments. Some key points from Lend Lease’s “Means and Methods of Asset Physical GMR’s” are listed below:

- Appropriate safety benching must be provided,
- Surrounding slopes should be no greater than 1:4 gradient,
- BBQ/picnic/play areas must not be within 20m of an unprotected waterway edge,
- Walkways on waterway edge of 2.5m min (preferable 3m+),
- Hard edge treatments (eg, piers, boardwalk) higher than 1m from the water should have a handrails,
- Appropriate safety and warning signage must be provided.
- Minimum freeboard of 600mm & 300mm to lots being created in subdivision.
- 1 in 100yr flood line on all typical cross sections and relevant minimum freeboard to lots
- Shared path material & design detail to the satisfaction of the Responsible Authority
- Maintenance agreement for waterway edge treatment will be entered in to by Melbourne Water and Wyndham City.

- Linear parks and waterways
- Water edge type 1 or 2 (hard)
- Water edge type 3, 4 or 5 (rock)
- Water edge type 6 or 7 (soft)



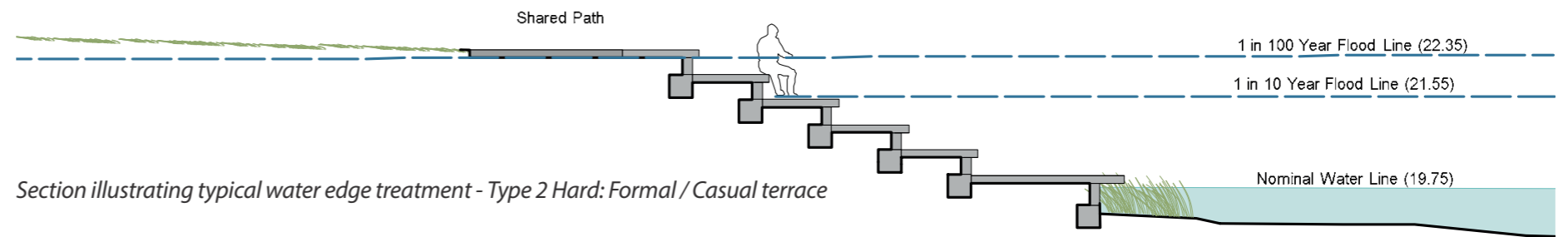
The different water bodies and their proposed edge treatments

**Typical Hard Edge Treatments**



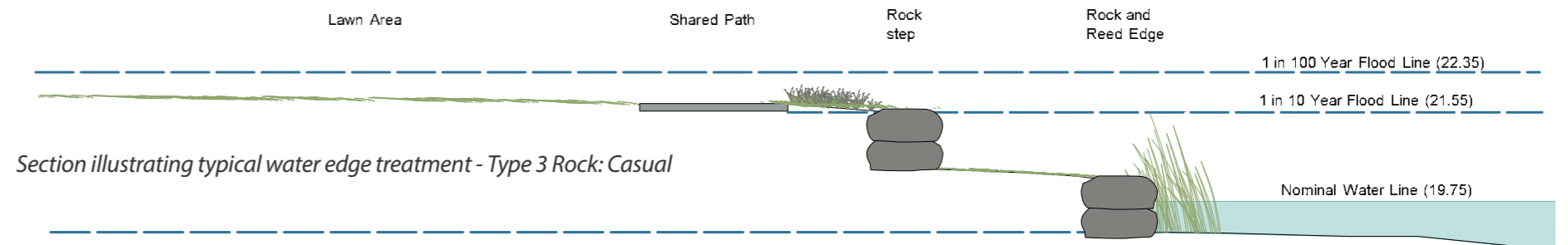
Section illustrating typical water edge treatment - Type 1 Hard: Formal Pier Structure

Note: Fencing must be provided to relevant Australian Standards.

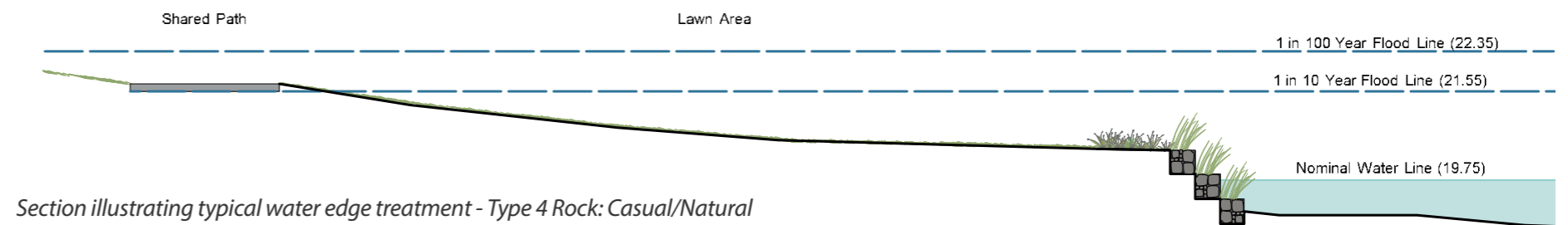


Section illustrating typical water edge treatment - Type 2 Hard: Formal / Casual terrace

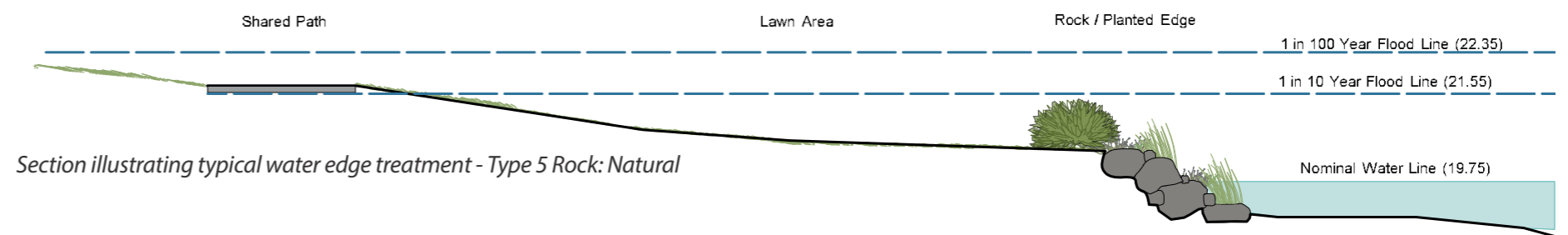
**Typical Rock Edge Treatments**



Section illustrating typical water edge treatment - Type 3 Rock: Casual

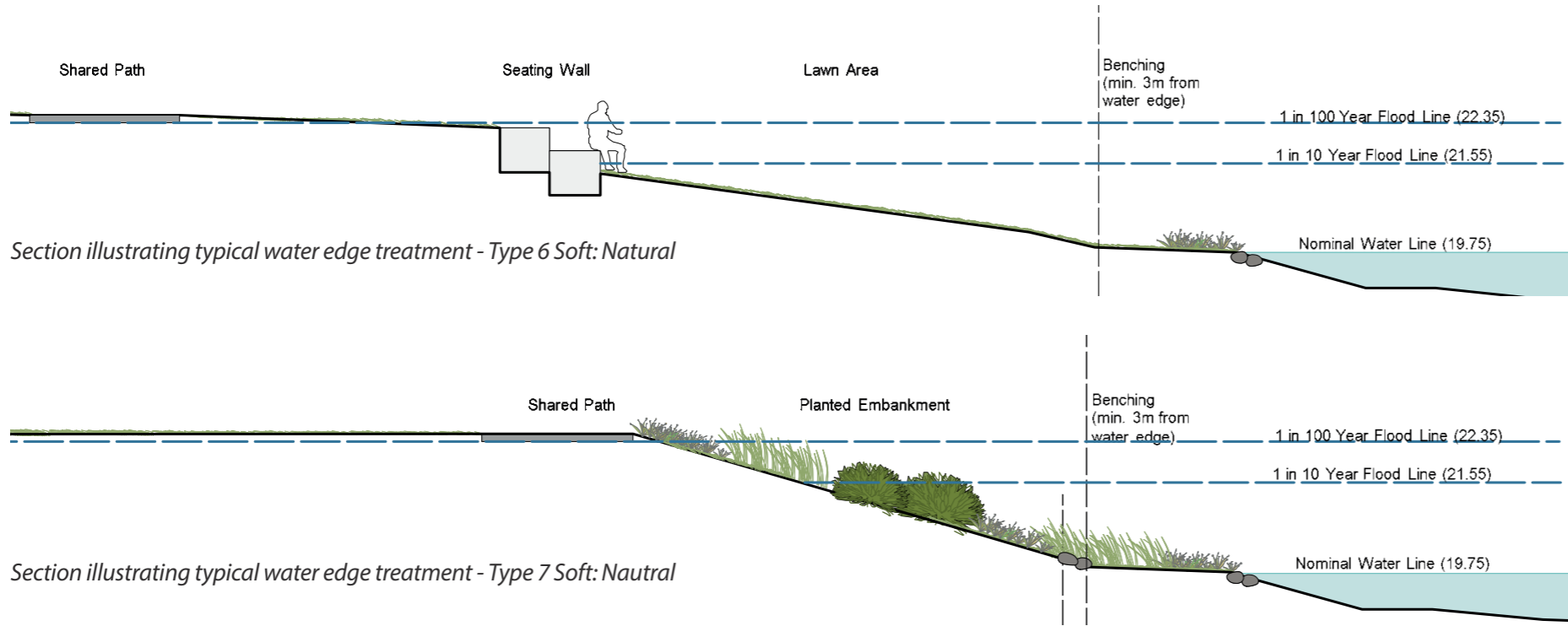


Section illustrating typical water edge treatment - Type 4 Rock: Casual/Natural



Section illustrating typical water edge treatment - Type 5 Rock: Natural

**Typical Soft Edge Treatments**



Section illustrating typical water edge treatment - Type 6 Soft: Natural

Section illustrating typical water edge treatment - Type 7 Soft: Natural

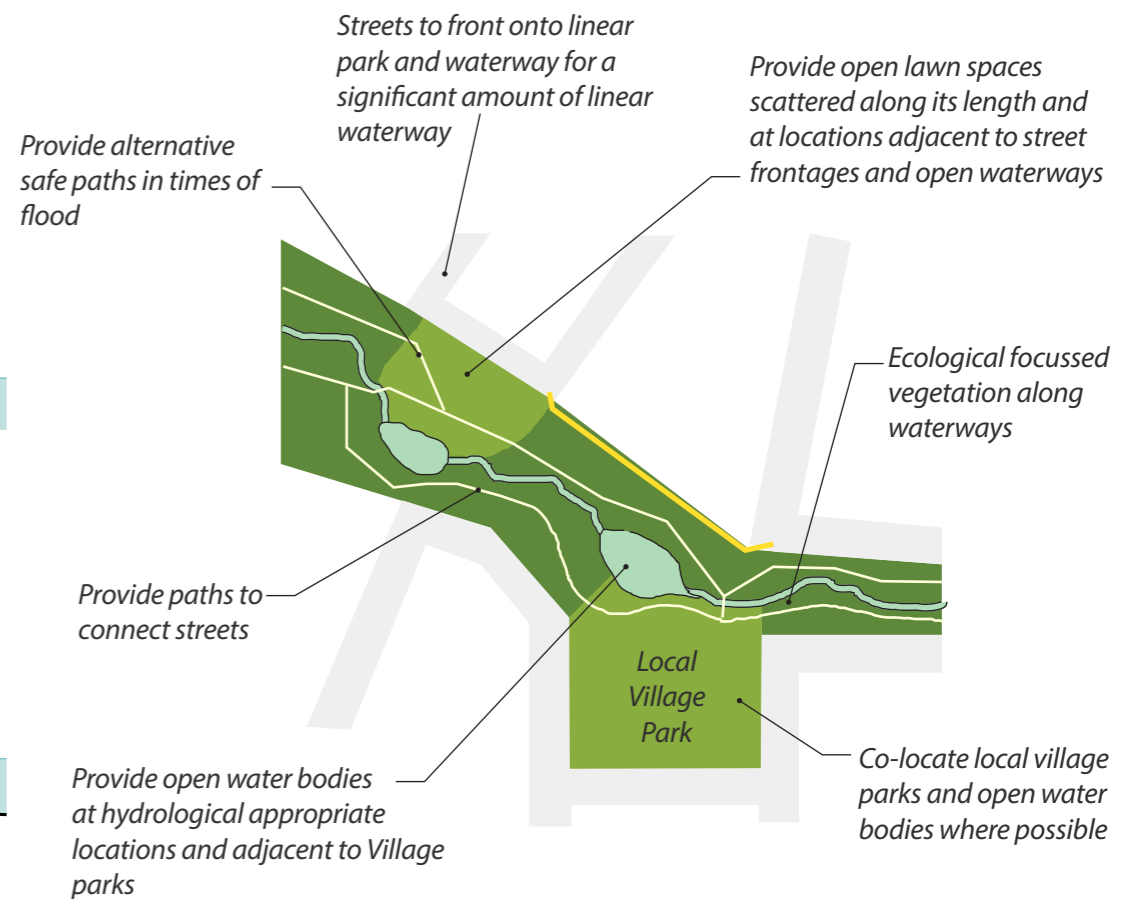
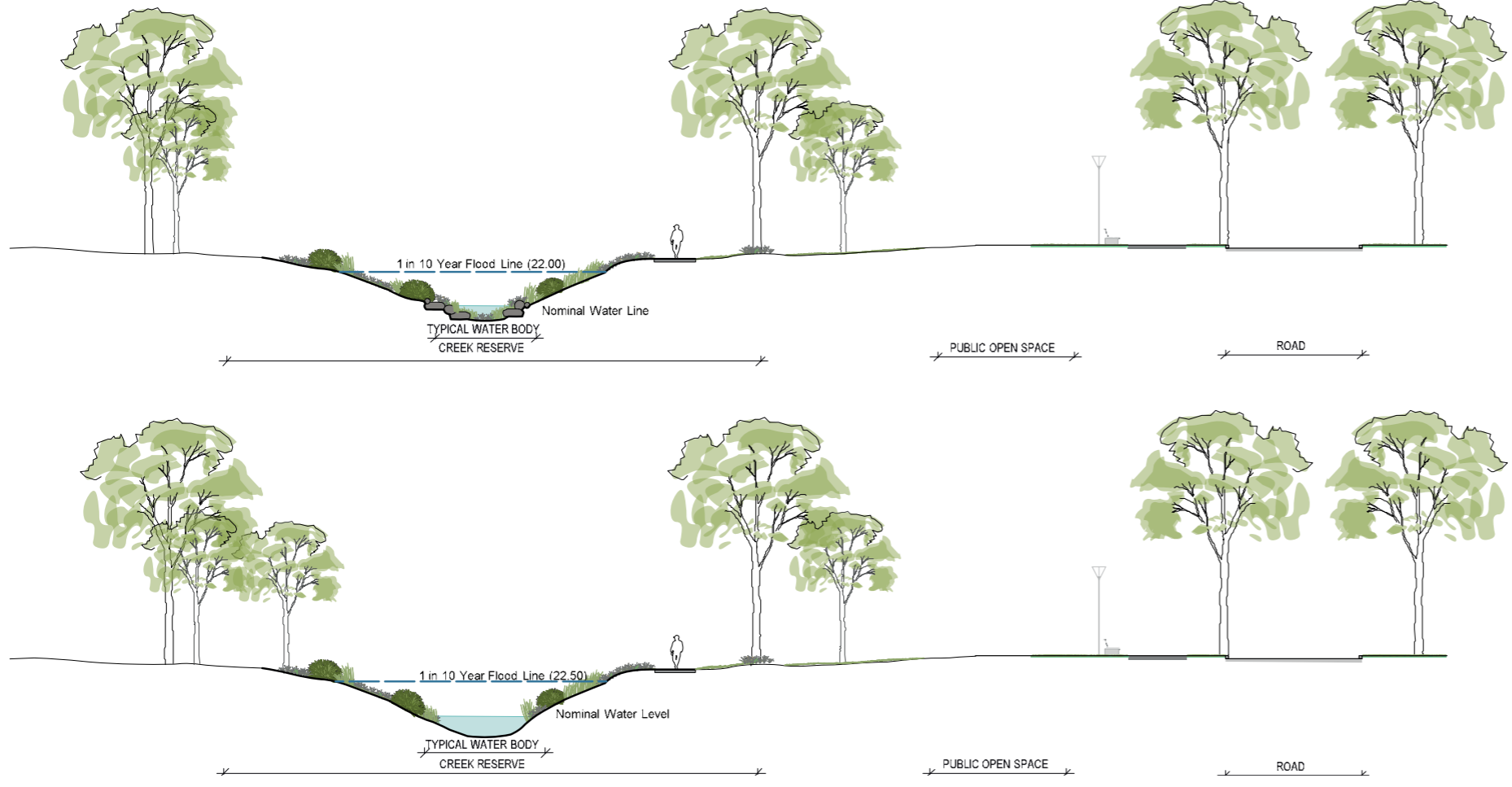


Figure 2: Typical layout of a linear park & waterway, areas of open lawn, paths & streets

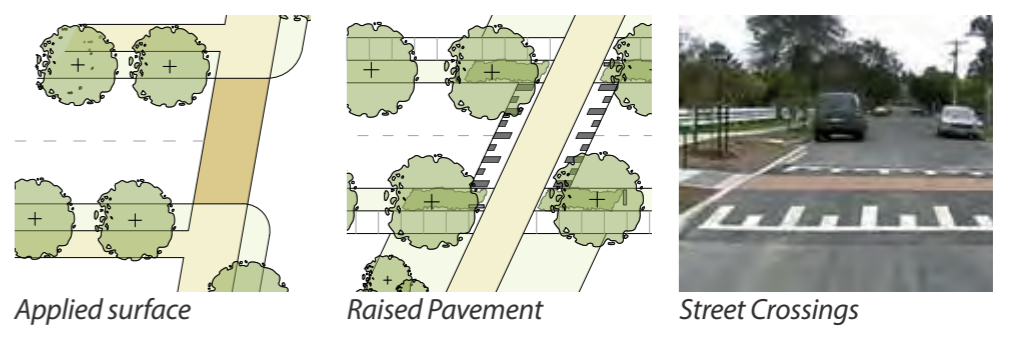
**Linear Parks and Waterways - Design and Edge Treatments**



Indicative cross sections of linear parks and waterways



Figure 3: Linear Park shared path treatment over streets



## 4.2 TOWN CENTRE

### Civic Gathering Space

Site Features / Description:

This park is anchored off the Town Centre main street 'promenade'. It is a critical space that integrates the town centre and the community with the waterways. The open space is connected to an internal shared path network 10km+ around the waterbody, which will link into the regional Lollipop Creek trail.

This space presents the opportunity for an iconic, regional social / family destination place. A location for public gathering and an event space.

### Size

- Village park of 1 Ha
- Encumbered usable space = 0.4 Ha
- Total effective usable area = 1.4 Ha



Amphitheatre



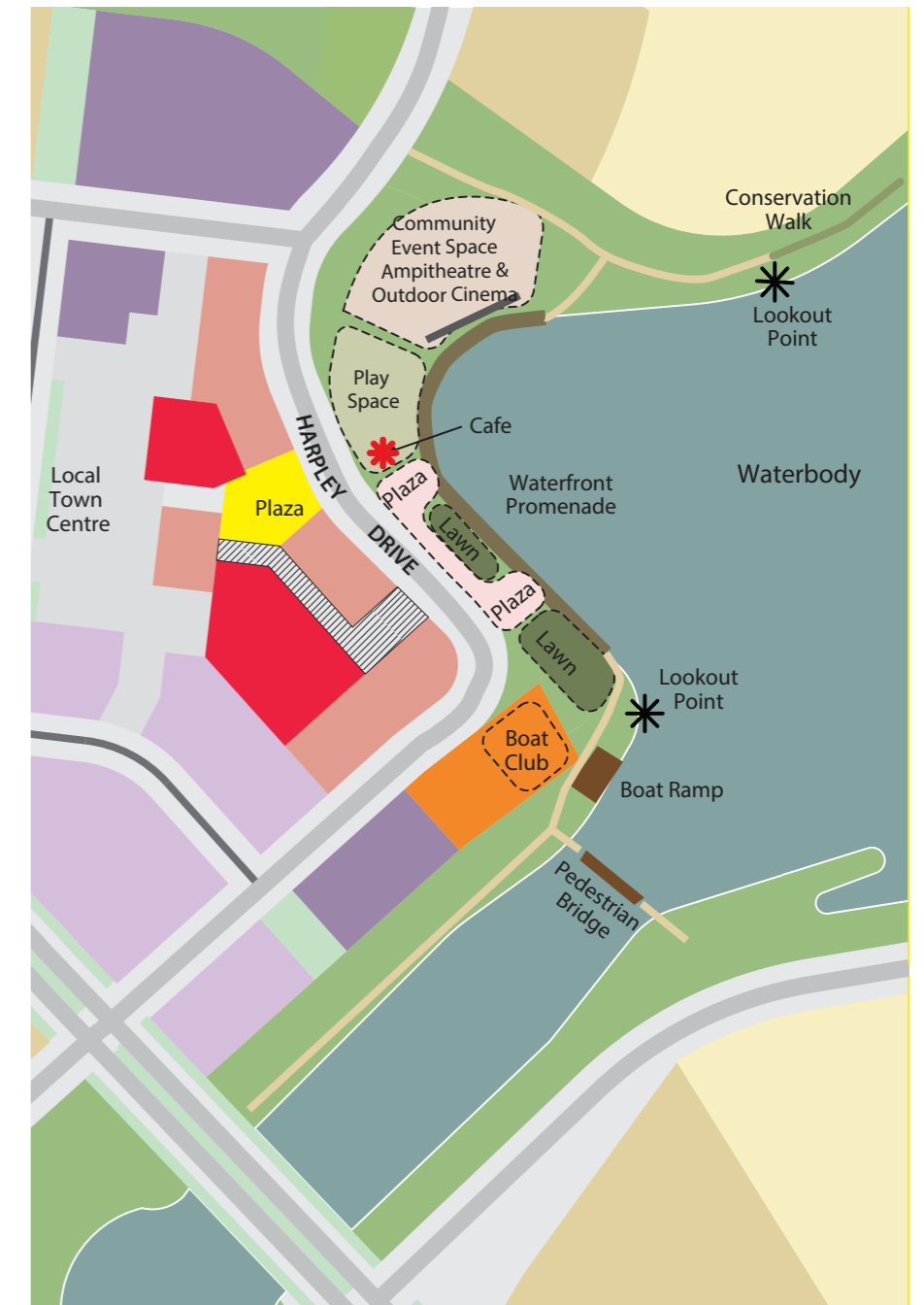
Terraces



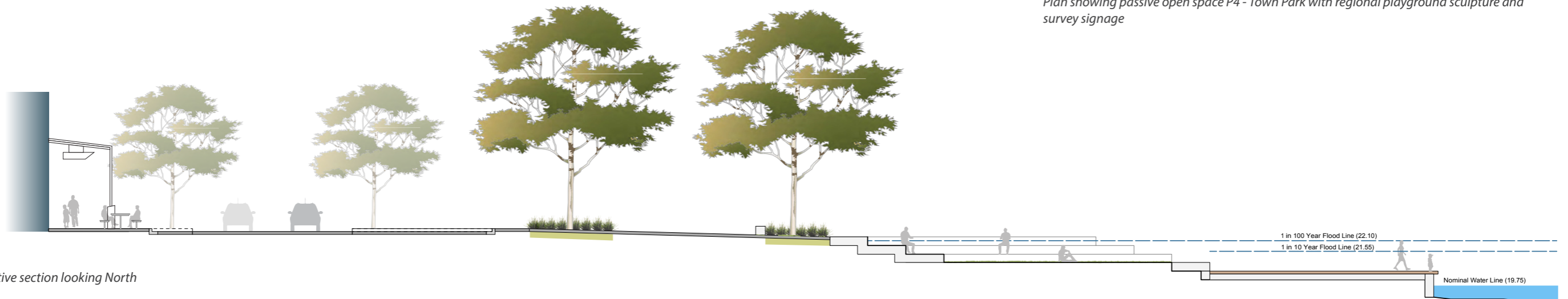
Playground



Formal Plaza with open air cinema



Plan showing passive open space P4 - Town Park with regional playground sculpture and survey signage



Indicative section looking North

**Space and functional requirements:**

- Gathering space
- Public art
- Shelters
- BBQ's
- Youth activity
- Amphitheatre with deck/stage
- Regional playground (opportunity for water play)
- Market place / plaza
- Cafe
- Tavern
- Car Park
- Picnic area with shelter, seating and BBQ
- Shared trail
- Timber promenade

**Design Principles**

- Combination of interface treatments, surveillance of site, safe movement through site when flooded
- Safety and flooding
- Alternative routes when in flood

**PLACE MAKING**

Place making is important for creating character and unique identity within a community. Three opportunities identified for place making are interpretation, wayfinding and community development.

**Interpretation**

Interpretive signage will be incorporated at key moments to reveal significance of historical, cultural and environmental aspects of the area.

It will provide the opportunity to develop places and spaces that express and celebrate community cultural identity and enable innovative arts practice. Such art should include historical and cultural interpretation, signage, path graphics, Aboriginal arts practice, celebration of the agricultural character of the area and so on.

- Environmental
- Cultural
- Historical



Public art sculpture



A shared pathway with distinct identity and visual grammar



Parc Des Locomotives, Fauteux et associes



Southport Broadwater Parklands, Queensland



McClelland Sculpture Survey Signage



Wayfinding signage



Information about storm water treatment



Bellarine Peninsula wayfinding and information page



Community Gardens and fruit tree groves



Destination places of cultural meaning



All weather shade structures



Outdoor furniture Strootman Landscape Architects

**Wayfinding**

Wayfinding signage will be iconic and robust. It will include: area maps, time and distance information and will assist users to orientate and journey plan.

Legibility and identity can assist with navigation, accessibility and understanding. This is especially important when identifying cultural significance urban character and destinations.

Consideration be given to utilising "wayfinding" signage to highlight of walk and ride to convey healthy lifestyle information

ie. 10 minute walk to x = 200 calories

- Signage
- Branding

**Community Development**

Developing informal spaces for meeting and community activity including community gardens shared facilities and places of significance.

- Community gardens - Providing land for the community to grow food and share space. Creating an edible landscape will bring people together, foster a sense of place, increase cultural understanding and provide education experiences.
- Encouraging relationships with other successful community groups (eg CERES)
- Shared facilities - Gathering or restful spaces to foster informal meetings and workshops/events and skill sharing.

## 4.3 LOCAL TOWN CENTRE DESIGN PRINCIPLES

### Principle 1

*Provide every neighbourhood with a viable Local Town Centre as a focus of the community with a fine grain, closely spaced distribution pattern.*

#### Guidelines

- Deliver a fine grain distribution pattern of highly accessible Local Town Centres generally on a scale of one Local Town Centre for every neighbourhood of 8,000 to 10,000 people.
- Locate Local Town Centres with a distribution pattern of around one Local Town Centre for every square mile (2.58km<sup>2</sup>) of residential development.
- Deliver a network of economically viable Local Town Centres including a supermarket and supporting competitive local shopping business, medical, leisure, recreation and community needs while allowing opportunities for local specialisation.

### Principle 2

*Locate Local Town Centres on a connector street intersection with access to an arterial road and transit stop.*

#### Guidelines

- Locate the Local Town Centre on an arterial/connector intersection and ensure that the Local Town Centre is central to the residential catchment that it services while optimising opportunities for passing trade.
- Locate the Local Town Centre with future railway stations or other forms of transit stops to benefit the Local Town Centre and to offer convenience for public transport passengers.
- Other Local Town Centre locations may be considered where the location results in the Local Town Centre being central to the residential catchment that it serves and/or the location incorporates natural or cultural landscape features such as rivers and creeks, tree rows, topographic features or other heritage structures which assist in creating a sense of place.

### Principle 3

*Locate Local Town Centres in an attractive setting so that most people live within a walkable catchment of a Local Town Centre and relate to the centre as the focus of the neighbourhood.*

#### Guidelines

- Ensure that 80-90% of households are within a 1km walkable catchment of a local or higher order Town Centre.
- Locate Local Town Centres in attractive settings and incorporate natural or cultural landscape features such creeks and waterways, linear open space, pedestrian and cycle links and areas of high aesthetic value.
- The design of the Local Town Centre should respect existing views and vistas to and from the Local Town Centre location.

### Principle 4

*Provide a full range of local community and other facilities including a supermarket, shops, medical and recreation uses.*

- Land uses should be located generally in accordance with the locations and general land

use terms identified on the Local Town Centre Concept Plan.

- The design of the Local Town Centre should facilitate development with a high degree of community interaction and provide a vibrant and viable mix of retail, recreation and community facilities.
- The creation of land use precincts within the centre is encouraged to facilitate the clustering of uses. For example a 'medical precinct' where similar or synergistic uses should be sited together to promote stronger trading patterns.
- The design of the Local Town Centre should also encourage a pattern of smaller scale individual tenancies and land ownership patterns within the Local Town Centre to attract investment and encourage greater diversity and opportunities for local business investment.
- The Local Town Centre should generally be anchored by one full line supermarket and supported by specialty stores unless otherwise noted on the Local Town Centre Concept Plan.
- Supermarkets and other commercial or community anchors or secondary anchors within the Local Town Centre should be located diagonally opposite one another across the main street and/or town square to promote desire lines that maximise pedestrian movement within the public realm.
- A small access mall that address a supermarket/other 'large box uses' may be considered as part of the overall design. Such access malls may have a limited number of internalised shops. The primary access to the mall should be from the main street and/or the town square.
- Active building frontages should address the main street and town square to maximise exposure to passing trade, and promote pedestrian interaction.
- Shopfronts should have varying widths and floor space areas to promote a diversity of trading opportunities throughout the Local Town Centre.
- Flexible floor spaces (including floor to ceiling heights) should be incorporated into building design to enable localised commercial uses to locate amongst the activity of the Local Town Centre.
- Mixed Use precincts should provide retail and/or office at ground level, and office, commercial and residential above ground level.
- Childcare, medical centres and specialised accommodation (e.g. aged care/nursing home, student accommodation, and serviced apartments) should be located within the Local Town Centre and at the edge of the Local Town Centre to contribute to the activity of the centre and so these uses are close to the services offered by the centre.
- Car parking areas should be located centrally to the site and to the rear and or side of street based retail frontages.
- Car parking areas should be designed to accommodate flexible uses and allow for long term development opportunities.
- Public toilets should be provided in locations which are safe and accessible and within the managed area of the property.

### Principle 5

*Focus on a public space as the centre of community life.*

#### Guidelines

- A public space which acts as the central meeting place within the Local Town Centre must be provided. This public space may take the form of a town square, town park, public plaza space, public market place or a similar locally responsive option.
- The public space should be located in a position where the key uses of the Local Town Centre are directly focuses on this public space to ensure that it is a dynamic and activated space.
- The public space should be designed to function as the identifiable 'centre' or 'heart' with a distinctive local character for both the Local Town Centre and the broader residential catchment.
- The public space should be designed as a flexible and adaptable space so that a range of uses can occur within this space at any one time. Such uses may include people accessing their daily shopping and business needs as well as providing a space where social interaction, relaxation, celebrations and temporary uses (such as stalls, exhibitions and markets) can occur.
- The public space should be well integrated with pedestrian and cycle links around and

through the Local Town Centre so that the public space acts as a 'gateway' to the activity of the centre.

- The main public space or town square within the Local Town Centre should have a minimum area of 500sq m. Smaller public spaces which are integrated within the built form design, are surrounded by active frontages and facilitate high levels of pedestrian movement are also encouraged.
- Footpath widths within and around the public space as well as along the main street should be sufficient to provide for pedestrian and mobility access as well as provide for outdoor dining and smaller gathering spaces.

### Principle 6

*Integrate local employment and service opportunities in a business friendly environment.*

#### Guidelines

- A variety of employment and business opportunities should be planned through the provision of a broad mix of land uses and commercial activities.
- A range of options and locations for office based businesses should be provided within the Local Town Centre.
- Services and facilities to support home based and smaller businesses are encouraged within the Local Town Centre.
- Appropriate locations for small office/home office ('SOHO') housing options which maximise the access and exposure to the activity of the Local Town Centre should be considered as part of the design process.

### Principle 7

*Include a range of medium and high density housing and other forms of residential uses within and surrounding the Local Town Centre.*

#### Guidelines

- Medium and high density housing in and around the Local Town Centre is required to provide passive surveillance, contribute to the life of the centre and to maximise the amenity of the centre.
- Medium and high density housing should establish in locations of high amenity around the Local Town Centre and be connected to the activity of the Local Town Centre through strong pedestrian and cycle links.
- A range of housing types for a cross section of the community (such as retirement living) should be included in and around the Local Town Centre.
- Specialised accommodation (such as aged/nursing care, student accommodation and serviced apartments) is encouraged at the edge of Local Town Centres with strong pedestrian and cycle links to the central activity area of the Town Centre.
- The Local Town Centre design should avoid potential land use conflicts between residential and commercial uses by focusing on retail operations on the main street and around the town square and locating residential uses predominantly at the edge of the Local Town Centre and/or on upper levels.
- Refer to the Small Lot Housing Code for further information about housing requirements for small lots around Local Town Centres.

### Principle 8

*Design the Local Town Centre to be pedestrian friendly and accessible by all modes including public transport, while enabling private vehicle access.*

#### Guidelines

- The Local Town Centre should be easily, directly and safely accessible for pedestrians, cyclists, public transport modes, private vehicles, service and delivery vehicles with priority given to pedestrian movement, amenity, convenience and safety.
- The Local Town Centre should provide a permeable network of streets, walkways and public spaces that provide linkages throughout the centre and designated pedestrian crossing points.
- The main street should be designed to comply with the relevant cross sections found within the Precinct Structure Plan.
- A speed environment of 40km/h or less should be designed for the length of the main street.
- Public transport infrastructure/facilities should be planned for commuter friendly/ convenient locations within the Local Town Centre.
- Bus stops should be provided in accordance with the Department of Transport Public Transport Guidelines for Land Use and Development, to the satisfaction of the Department of Transport.
- Bicycle parking should be provided within the street network and public spaces in highly visible locations and close to pedestrian desire lines and key destinations.
- Supermarket and other 'large format' buildings should not impede on the movement of people around the Local Town Centre.
- Key buildings within the Local Town Centre should be located to encourage pedestrian movement along the length of the street through public spaces.
- The design of buildings within the Local Town Centre should have a relationship with and should interface to the public street network.
- Car parking areas should be designated to ensure passive surveillance and public safety through adequate positioning and lighting.
- Car parking areas should be designed to provide dedicated pedestrian routes and areas of landscaping.
- On street car parking should be provided either as parallel or angle parking to encourage short stay parking.
- Car parking ingress and egress crossovers should be grouped and limited.
- Car parking ingress or egress and car parking areas accommodating heavy vehicle movements should be designed to limit the pedestrian/vehicle conflict.
- Heavy vehicle movements (i.e. loading and deliveries) should be located to the rear and or side of street based retail frontages
- Streets, public spaces and car parks should be well lit to Australian standards and with pedestrian friendly (generally white) light. Lighting should be designed to avoid unnecessary spill to the side or above.
- All public spaces should respond appropriately to the design for mobility access principles.

## Principle 9

*Create a sense of place with high quality engaging urban design.*

### Guidelines

- Development should complement and enhance the character of the surrounding area by responding appropriately to key visual cues associated with the topography of the Local Town Centre location and its surrounds.
- The Local Town Centre design should seek to minimise amenity and noise impacts resulting from the mix of uses by maintaining separation and transitional areas between retail and housing activities, such as open space, road networks and community facilities.
- The design of each building should contribute to a cohesive and legible character for the Local Town Centre as a whole.
- Sites in prominent locations (such as at key intersections, surrounding public spaces and terminating key view lines and vistas) should be identified for significant buildings or landmark structures.
- The design of building frontages should incorporate the use of a consistent covered walkway or verandah to provide for weather protection.
- The built form should define the main street and be aligned with the property boundary.

- Street facades and all visible side or rear facades should be visually rich, interesting and well articulated and be finished in suitable materials and colours that contribute to the character of the Local Town Centre.
- Corner sites, where the main street meets an intersecting and/or arterial road should:
  - Be designed to provide built form that anchors the main street to the intersecting road. This can be achieved through increased building height, scale and articulated frontages;
  - Incorporate either 2 storey building or 2 storey elements (such as awnings and roof lines);
  - Be developed to have a ground floor active frontage and active floor space component to the main street frontage;
  - Not be developed for standard single storey fast food outcomes.
- Materials and design elements should be compatible with the environment and landscape character of the broader precinct.
- The supermarket and secondary anchors should have frontages that directly address the main street and/or town square so that the use integrates with and promotes activity within the main street and public spaces/thoroughfares.
- Supermarkets or large format retail uses with a frontage to the main street should use clear glazing to allow view lines into the store from the street. (Planning permits for buildings and works should condition against the use of white washed windows, excessive window advertising and obtrusive internal shelving or 'false walls' offset from the glazing).
- Secondary access to the supermarket from car parking areas should be considered where it facilitates convenient trolley access and does not diminish the role of the primary access from the main street and or town square.
- The design and siting of supermarkets and other 'large format retail uses' should provide an appropriate response to the entire public domain. This includes but is not limited to car parking areas, predominantly routes and streets.
- Retail uses along street frontages should generally include access points at regular intervals to encourage activity along the length of the street.
- Retail and commercial buildings within the Local Town Centre should generally be built to the property line.
- Public spaces should be oriented to capture north sun and protect from prevailing winds and weather.
- Landscaping of all interface areas should be of a high standard as an important element to complement the built form design.
- Urban art should be incorporated into the design of the public realm.
- Street furniture should be located in areas that are highly visible and close to or adjoining pedestrian desire lines/gathering spaces and designed to add visual interest to the Local Town Centre.
- Wrapping of car parking edges with built form, to improve street interface, should be maximised.
- Car parking areas should provide for appropriate landscaping with planting of canopy trees and dedicated pedestrian thoroughfares.
- Screening of centralised waste collection points should minimise amenity impacts with adjoining areas and users of the centre.
- Where service areas are accessible from car parks, they should present a well designed and secure facade to public areas.
- Mechanical plant and service structure roofs should be included within roof lines or otherwise hidden from view.

## Principle 10

*Promote localisation, sustainability and adaptability.*

### Guidelines

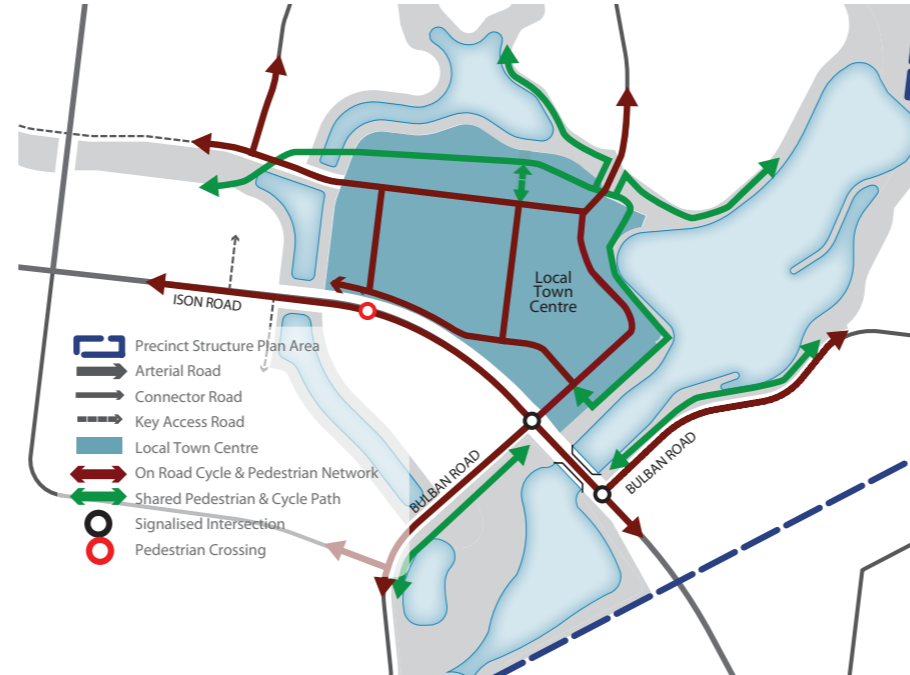
- The Local Town Centre should promote the localisation of services which will contribute to a reduction of travel distance to access local services and less dependence on the car.
- The Local Town Centre should be designed to be sympathetic to its natural surrounds by:
  - Investigating the use of energy efficient design and construction methods for all buildings;
  - Including Water Sensitive Urban Design principles such as integrated stormwater retention and reuse (e.g. toilet flushing and landscape irrigation);
  - Promoting safe and direct accessibility and mobility within and to and from the Local Town Centre;
  - Including options for shade and shelter through a combination of landscape and built form treatments;
  - Ensuring buildings are naturally ventilated to reduce the reliance on plant equipment for heating and cooling;
  - Promoting passive solar orientation in the configuration and distribution of built form and public spaces;
  - Grouping waste collection points to maximise opportunities for recycling and reuse;
  - Promoting solar energy for water and space heating, electricity generation and internal and external lighting; and
- Investigating other opportunities for the built form to reduce greenhouse gas emissions associated with the occupation and the ongoing use of buildings.
- Encourage building design which can be adapted to accommodate a variety of uses over time.
- Ensure the Local Town Centre has an inbuilt capacity for growth and change to enable adaptation and the intensification of uses as the needs of the community evolve.



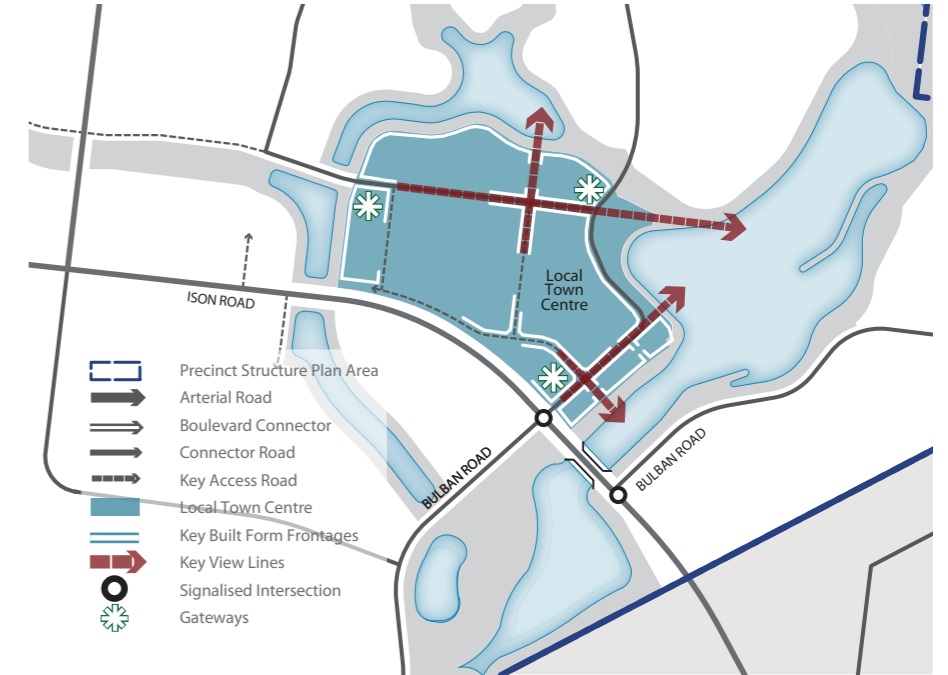
Organising Element Plans



The LTC is positioned central to the residential catchment and maximises the amenity offered by the water bodies and wetland areas.



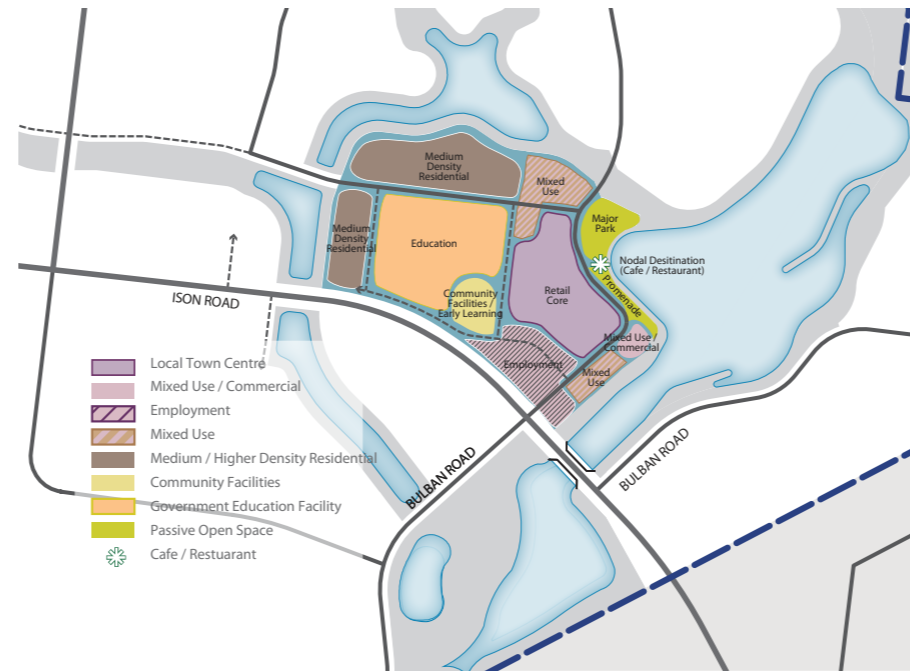
The LTC design maximises view lines and vistas to the water bodies and wetlands while maintaining access to Ison Road.



The positioning of land uses creates a centre with strong relationships to the water bodies and wetlands while creating an active mixed use LTC



The parkland surrounding the LTC will connect the activity of the centre with the amenity of the water bodies and wetlands creating a major open space destination for the community



LTC Precinct Plan

## 4.4 PLANTING GUIDELINES

### Street Tree Design Guidelines

Trees in parks and streets are an essential component of the development; they provide social, economic and environmental benefits to the community.

#### Key Benefits of Successful Street tree planting

- Contribute to reducing the heat-island effect in our urban areas, by reducing the rate of precipitation and by providing shade
- Provide amenity and character for the community
- Informs overall identity and way finding through the development
- Support local wildlife and general biodiversity
- Improve the health and wellbeing of residents and visitors, through increased recreation, walking, socialisation on streets and the overall benefits of trees in reducing heat and dust impacts
- Reduces the use of air conditioning within houses
- Encourages non-vehicle use of streets
- Increase the value of properties in streets with broad canopy and lush tree planting

#### Guidelines for Street Trees Selection

- Provide the largest canopy tree possible appropriate for the street function and scale
- Ensure species will perform in the long term for their climatic location and site conditions. In this case choose trees that will thrive within dry conditions, in local soils, and able to resist wind damage
- Choose a range of species and reduce monoculture, to reduce the potential loss of trees though disease and climate
- Choose species that will create a strong identity and character for the street
- Choose species that can provide habitat and food for local flora and fauna
- Ensure species selected are also compatible for water sensitive urban design systems (WSUD)
- Choose species that have acceptable council maintenance requirements.

### Tree placement and Streetscape Design

- Indented parking bays with trees in between
- Interlocking and overlapping tree canopy cover rather than just parking lanes
- Active multiple tree placement in any given street cross section
- Break up expanse of asphalt
- Passive irrigation of street trees where possible

### Preferred Streetscape Design



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[www.gaa.vic.gov.au](http://www.gaa.vic.gov.au)

*partners in creating new communities*